

Newquay Character Study

Introduction	2
How to use this Document	2
Links to other character assessments and design guidance	3
1: Central Commercial Core	4
2: Harbour related residential	8
3: The Harbour	11
4a: Industrial area (The Whim)	14
4b: Industrial Area Wesley Yard	16
4c: Industrial area – land around the Railway Station	18
4d: Industrial area [incorporated into the survey for 10a]	20
4e Industrial area – The Tramway	21
5: Secondary Commercial West	24
6: Secondary Commercial area East [East Cliff and Cliff Road]	27
7: Towan Promenade and Killacourt	31
8: Towan head and Fistral beach	33
9, 9a: Narrowcliff and Barrowfields	37
10a: Mount Wise residential and 4d Industrial site	40
10b: South East part of CSUS area 10	43
11: Trenance Park	45
12: Gannel Side	48
13: Pentire and Esplanade Roads	51
14: Lawton Close	54
15: Pentire – Gannel Facing	56
16a: Penmere	58
16b: Tregunnel Hill [south] and Estuary View	60
17: Tregunnel Hill [Higher]	62
18: Mayfield	64
19: Treloggan	67
20a: Tretherras South: Towan Blystra estates and Trencreek	69
20b: Trevenson Meadows [east of Trevenson Road]	71
21: Tretheras North and adjacent 10 (part) Residential Area within CSUS	73
22: Henvor Road	76
23: Porth Veor	79
24: Porth	82
25: Porth Bean	84
26: St Columb Minor	86
27a: Whipsiderry	89
27b: Tregurrian	91
28: Bezant Place and Hatfield Crescent	93
29: Tregunnel Hill : Duchy site	95
30: Nansledan	98

Introduction

This document is an evidence document to support the Newquay Neighbourhood Plan and particularly the policies on general development, design and heritage conservation. It provides a summary of evidence from a range of sources, including recent community surveys, that identify the special characteristics of the built environment in Newquay and its relationship to the natural environment.

The Newquay Character Study identifies the most important characteristics of each area, key views from and through the area. It sets out some key design and conservation principles particularly relevant to that area and makes suggestions where future improvement initiatives could be developed.

For the purposes of this study, the settlement of Newquay has been divided into character areas with a summary produced for each. Some of these character areas were subject to a thorough assessment of their historic character in the 2003 publication “The Cornwall and Scilly Urban Survey for Newquay” (CSUS). In these areas, any significant changes since its original publication have also been identified. Further references have been made to the Duchy of Cornwall’s Pattern book for Newquay, hereafter referred to as the “Newquay Pattern Book” produced to support the design of the growth areas. Additional references have been made to planning application reports and Cornwall Design Review Panel reports.

The main contribution has been from specific community surveys for each area undertaken by volunteers plus contributions made by the plan steering group and consultant team in 2018. These original surveys are still retained as an evidence base but have been summarised in the following study. Prior to the surveys, the volunteer team were given workshop training on key principles of design and conservation and using a prepared methodology and template for recording characteristics.

How to use this Document

Firstly, you will need to identify the character area under consideration through a key map of the contents list in the report. A **brief description** for each individual cell is followed by a map, some photos and references to sections in the **Newquay Pattern Book** if relevant, and a list of **key changes** since the **CSUS study** if it is one of the 11 CSUS areas. The following sections identify special **Characteristics** and **Key views** which are important to protect. Then **Key design principles** set out some important guidance which should be applied in addition to standard good practice. **Opportunities for improvement** lists potential initiatives or focus areas for potential future projects. These are varied and could be initiated by Councils, agencies, community groups or through development.

Links to other character assessments and design guidance

Cornwall and Scilly Urban Survey (CSUS)	These areas are covered in the CSUS Newquay report. The early chapters of this report describe the historic development of the town and the character of the settlement. From page 31, separate descriptions are given of individual character areas including this area and a summary map. Other useful maps include historic OS maps and locations of surviving historic buildings.
Newquay Pattern Book and Design Code	The Newquay Pattern Book gives town planners, architects and builders a resource of typologies and details for the design of urban spaces and buildings that both stem from and reinforce local characteristics. A study of existing characteristics within Newquay was produced for the Duchy of Cornwall to guide new development in the growth areas but has much to offer anyone seeking to undertake work in the settlement of Newquay.
Cornwall Design Guide	The Cornwall Design Guide applies to new development and sets out the key elements of good design and makes recommendations for a good design process.
Shopfront Design Guidance	The Cornwall Shopfront Design Guide is very relevant to apply in this area both for restoration of historic shopfronts and contemporary design. This is accompanied by a special Shopfront Appendix for Newquay highlighting special characteristics or current challenges with shops and signs in the town centre. It sets out some objectives and project ideas to improve the local shopping environment.
Designations	The Cornwall Council Interactive Map is a great online resource to identify local designations such as Listed Buildings, Historic Environments Records and Tree Preservation Orders.

1: Central Commercial Core

This area is the heart of Newquay and the heart of the historic village [known as the Manor of Towan Blystra until its sale in 1838]. It is the centre of commercial activity; its principal streets [Bank Street-Central Square-Fore Street] its trading centre and is thronged with summer tourists. Central Square was once a larger public space, later infilled and now survives as a triangular space at the junction of 3 streets.

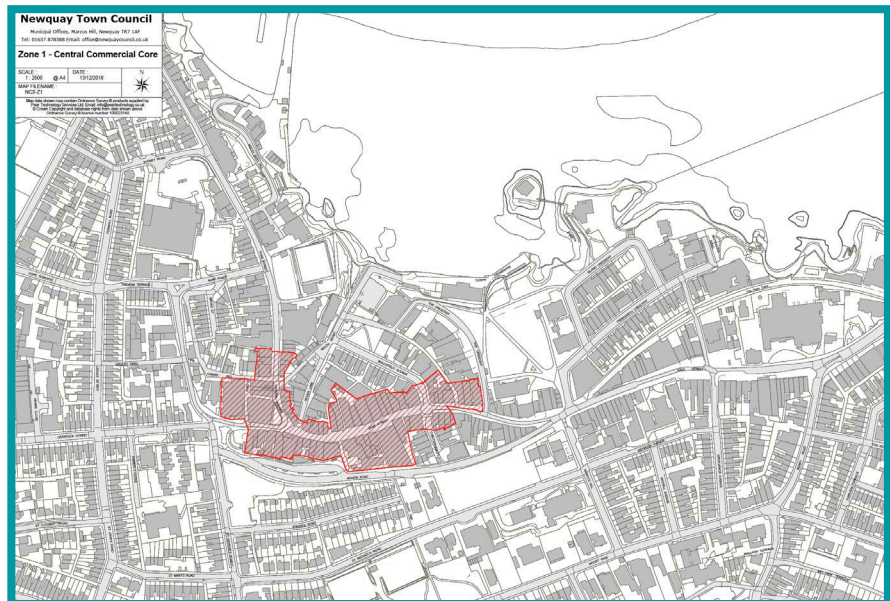


Left: view towards Fore St with important gateway buildings.



Right: Crossroads on Bank Street with important corner buildings.

General Description



Map extracted from the CSUS survey showing boundary of central core.



Key changes since CSUS study



Victoria Hall has been lost, originally fronting onto King Street and behind “The Central” pub, it was a concert hall cinema and community space.

The former Lloyds Bank has closed but has undergone many unsympathetic alterations to the frontage and signage affecting its special architectural composition.

Many shopfronts have been altered and signs overlaid on buildings gradually losing the special architectural quality of this Central area. The Lighthouse Cinema is the significant new building in this area, built on the site of the Victoria Hall.

The junction of Bank Street/Cheltenham Place/Beachfield Avenue has been redesigned and repaved in stone.

<p>Key changes since CSUS study <i>(cont.)</i></p>	<p><i>Mid-20th century photo of the Central Hotel and contemporary view below - all of the historic frontages viewed here are still retained under later alterations. This should be the focus for traffic control or removal and enhancement of the buildings and public realm. The road currently dominates the space.</i></p>  
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> • Central Square p15 • Town High Street p16 • Commercial Buildings p31 • Mixed use Buildings p32-34
<p>Important Characteristics of the Area</p>	<ul style="list-style-type: none"> • There are many historic buildings that retain fascinating architectural detail and design quality adding richness to the street. [many only recognised by looking above later shop alterations and signs] • There are many historic buildings that still retain detail and fabric that has been hidden by later alterations but could be revealed. • Strong urban enclosure [i.e. built directly on the back of pavement with no gaps other than the occasional street or narrow opeway] of these key streets, continuous frontages of mostly 3 storeys with occasional 2 storeys. Streets are relatively narrow. See Pattern Book. • There are many special buildings of high architectural quality, townscape value or interesting historical associations including: <ul style="list-style-type: none"> • The Central Inn, dominating Central Square, is a historic coaching inn. • The dramatic curved “angle iron” building on the Beach Road junction though poorly altered with lack of original or quality replacement shopfront and clutter at ground level.

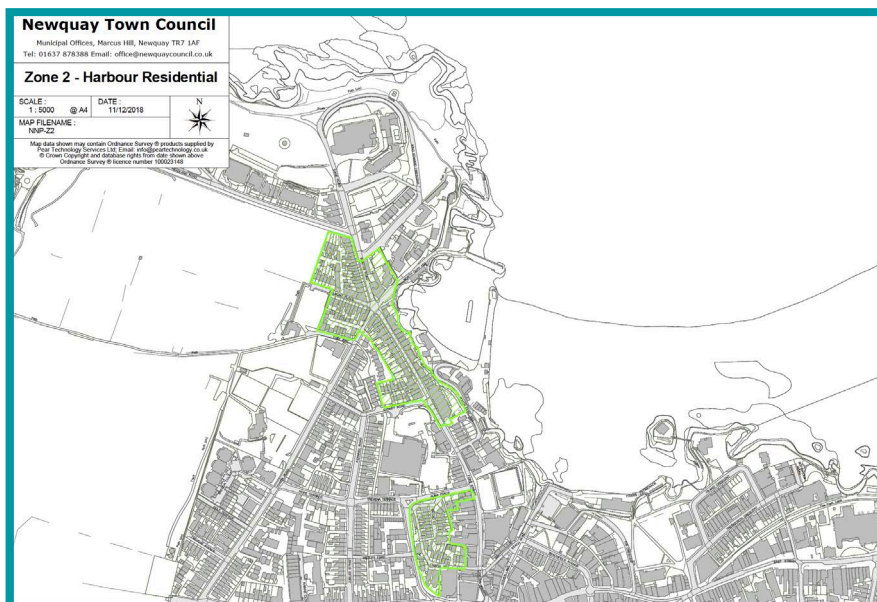
Important Characteristics of the Area <i>(cont.)</i>	<ul style="list-style-type: none"> • Very dramatic Edwardian building on the west side of Fore Street, 2-4 Fore street with “black and white” upper floor, cupola over the clock, and high quality tall ground floor shopfronts with stone clad pilasters and incorporating curved glass. • The central section of Bank Street has been pedestrianised with limited vehicular access giving more space for thronging shoppers and tourists. • Two Silvanus Trevellick buildings: 31 Bank Street corner building, and the former Lloyd’s bank on Central Square.
Key views to Protect	<ul style="list-style-type: none"> • The views to the water from the main street include Beach Road and Gover Lane. • The views of Central Square • The view towards Fore Street <div style="display: flex; justify-content: space-around; align-items: flex-end;">   </div> <div style="display: flex; justify-content: space-around; margin-top: 10px;"> <p><i>Left: view down Beach Road.</i></p> <p><i>Right: view down Gover Lane</i></p> </div>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Ensure the design principles in the Cornwall Shopfront Design Guide and the Newquay Appendix are followed in the undertaking of any restoration, replacement or new design, and including signage and shop adverts. This is critical to restore the quality of this priority area in Newquay. • Ensure no more loss of historic fabric in this area, restore and incorporate historic fabric, materials and detail into new work on buildings. • Maintain the scale of existing buildings in the central area including the height and the rhythm of plot widths. • Ensure all buildings retain an active frontage to the main streets and side streets and opeways. These should include ground floor windows and entrances. • Use the information in the Newquay Pattern Book to help recognise important features and guide restoration or new work.

Key Design Principles to adopt in the area <i>(cont.)</i>	<ul style="list-style-type: none"> • Clear the clutter. A comprehensive design scheme for the public realm must involve all public utility companies, highway authority and business owners to work together to sensitively design and locate necessary signage and infrastructure.
Opportunities for improvement	<ul style="list-style-type: none"> • Establish a programme of sensitive restoration of historic buildings and a principle of “no more loss of historic fabric”. This is an asset that could be exploited for the town. • Propose and support the designation of a conservation area in the town centre and the preparation of a Conservation Area Appraisal and Management Plan. • Promote the use of the Cornwall Shopfront Design Guide and the Newquay Shopfront Appendix in the town among local businesses, building owners and agents. • Reducing vehicular access on all these main streets, i.e. retain vehicular access only at set times for servicing at the beginning and end of the day. • Repave the streets with a more sympathetic design for its historic setting, and more accessible for all its pedestrian users. • The priority project should be to enable the delivery of a new comprehensive public realm redesign for Central Square as Newquay’s high quality urban space, bringing together building, traffic management and street design improvements.

2: Harbour Related Residential

General Description

These two areas (identified as 2a and 2b in CSUS) capture some of the character of the pre-urbanised fishing community. Includes some 18th century and possibly earlier domestic architecture and 19th and 20th century cottage rows and stone terraces. Some larger residencies and semi-exotic planting in front gardens. Includes the Red Lion – an important old coaching house with characteristic veranda.



Above: Fore Street looking South

Key changes since CSUS study



- Approximately 15 years ago (just post CSUS report) there was a hotel adjacent to Red Lion, now demolished and replaced by apartment building (21 2-bedroom flats).
- Old Victoria Cinema once opened onto Chapel Hill. Now replaced by the Lighthouse Cinema with no access from Chapel Hill.

Newquay Pattern Book

- p17 Village High Street
- p21 Town Residential
- p42 and 43 Two-bay houses
- p44 45 Cottages

Important Characteristics of the Area

- Open views from the Fore Street car park of the bay.
- Red Lion pub – old coaching Inn
- Broad Street - one of oldest streets in the town.

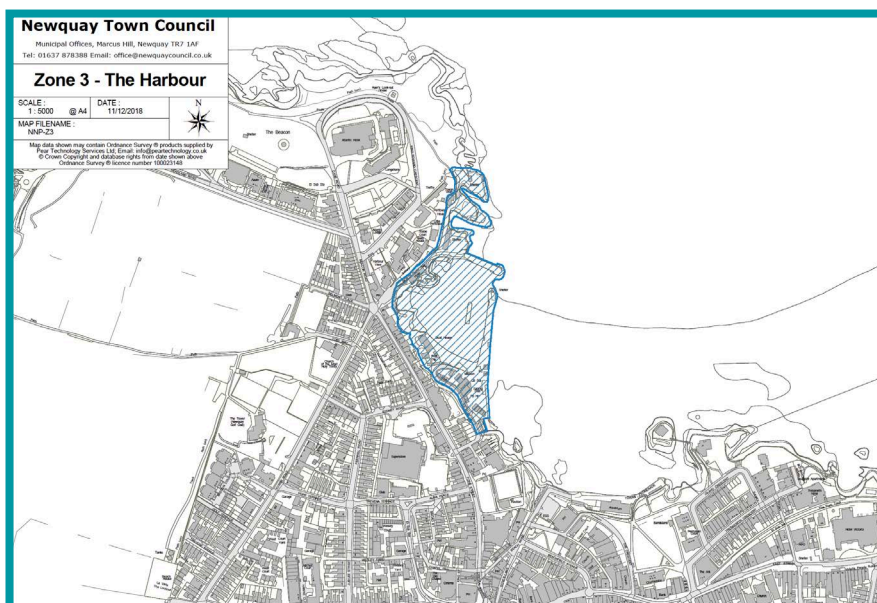
<p>Important Characteristics of the Area</p>	<ul style="list-style-type: none"> • Characteristic narrow terraced streets. • Generally, two storey buildings in terraces a tight enclosure of streets and spaces.  <p><i>Broad Street</i></p>
<p>Key views to protect</p>	<ul style="list-style-type: none"> • View of the memorial along Fore Street [recently compromised by Zinc Development] • Belmont Place looking towards the Golf Course over the car park and boundary hedge. • North Quay Hill by the Red Lion Forecourt with extensive views to the coastline.  <p><i>View North along Fore Street</i></p>
<p>Key Design Principles to adopt in the area.</p>	<ul style="list-style-type: none"> • Refer to the shopfront design guide for restoration, replacements and improvements to the design of shopfronts and signs. • Keep the local tradition of hand painting signage on rendered or painted brick walls rather than applying signs to buildings. • Ensure no more loss of historic fabric in this area, restore and incorporate historic fabric, materials and detail into new work on buildings. Restore special features such as timber sash windows, panelled doors, porches, pillars, walls and ironwork. • Ensure all buildings retain an active frontage to the main streets and side streets and opeways. These should include ground floor windows and entrances.

<p>Key Design Principles to adopt in the area. <i>(cont.)</i></p>	<ul style="list-style-type: none"> • Use the information in the Newquay Pattern Book to help recognise important features and guide restoration or new work. • Use traditional materials common to the area in improvement work to buildings, outside structures and landscape. • Any opportunities to create more enclosure and definition to the frontage on Alma Place and the design of its left-over spaces would be encouraged. • The residential streets typically have the pitch roof ridges running parallel to the street rather than gables facing the street.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • Enhance the Red Lion roundabout and forecourt • Propose and support the designation of a conservation area in the town centre and the preparation of a Conservation Area Appraisal and Management Plan. • Promote the use of the Cornwall Shopfront Design Guide and the Newquay Shopfront Appendix in the town among local businesses, building owners and agents. • The five ways roundabout at the Red Lion is a special feature and careful improvements could be made to the building frontages on the corners to make a dramatic and attractive space. <div data-bbox="584 1218 895 1628">  </div> <p><i>Above left: Granite pillars are a characteristic feature in front garden walls on Fore Street</i></p> <div data-bbox="932 1218 1422 1628">  </div> <p><i>Above right: Red Lion Pub and recent flats developed adjacent.</i></p>

3: The Harbour

The harbour forms a secluded and sheltered space set apart from the bustle of the town. It is comprised of robust stone built historic structures with more temporary timber framed buildings and additions. The cliff face and rubble revetment wall on the land side provide a strong sense of enclosure. It is overlooked by properties on Fore Street and Dane Road. Still a working environment with a fishing fleet and a catch focusing on crabs, shellfish and lobsters, with tripper boats and other private boats. It includes the RNLI headquarters and Seamans Mission.

General Description



Below: Newquay Harbour



Key changes since CSUS study

- Treffry Centre built to replace fishing storage shacks, burnt down, and provide additional office accommodation.
- Restoration of quays including the shelter plus new davits, new railings. Improvement work instigated by new marine management.
- Extension of the Rowing Club to provide new storage facilities for the historic gigs moving them from their previous storage in the Whim tunnel.

Important Characteristics of the Area	<ul style="list-style-type: none"> • South and North Quay listed grade II • Working fleet maintained • Small temporary buildings on robust stone structures • Community and visitor focus e.g. the Fish festival • The Whim Tunnel.
Key views to Protect	<ul style="list-style-type: none"> • Views from both quays across the harbour and back to the cliff face and to the town. • Views over South Quay Hill wall and low rooftops behind over the harbour and cliff face. <div data-bbox="563 725 975 1037" data-label="Image"> </div> <div data-bbox="1007 725 1422 1037" data-label="Image"> </div> <p><i>Spectacular views from both quays across the harbour.</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Maintain the small-scale construction of any new or replacement structures on or next to the quays. Slated pitched roofs work well here and timber-framed and clad construction can be used as an alternative to dressed stone for more permanent structures. • Retain and repair all stone walls and structures in the area which are an important characteristic. • Ensure the area remains safe and accessible to the public as well as the working community and ensure any designs for safety or signage are done sensitively.
Opportunities for improvement	<ul style="list-style-type: none"> • Propose and support the designation of a conservation area in the town centre and the preparation of a Conservation Area Appraisal and Management Plan. • Promote the use of the Cornwall Shopfront Design Guide and the Newquay Shopfront Appendix in the town among local businesses, building owners and agents for signage and advertising around the harbour. • Repair restoration work to the cliff face will be required where there have been recent falls - this needs to be undertaken sensitively as well as practically – awaiting funding opportunities.

Opportunities for improvement *(cont.)*

- Addition of low water landing stage.
- Potential for the re-opening of the Whim Tunnel.
- Potential for improvements to car parking.
- Potential for better gateways and routes from the town above e.g. from South Quay to Towan. Include the potential for improving the design of incidental areas and structures along South Quay Hill leading down to the harbour and maintain views over the wall.

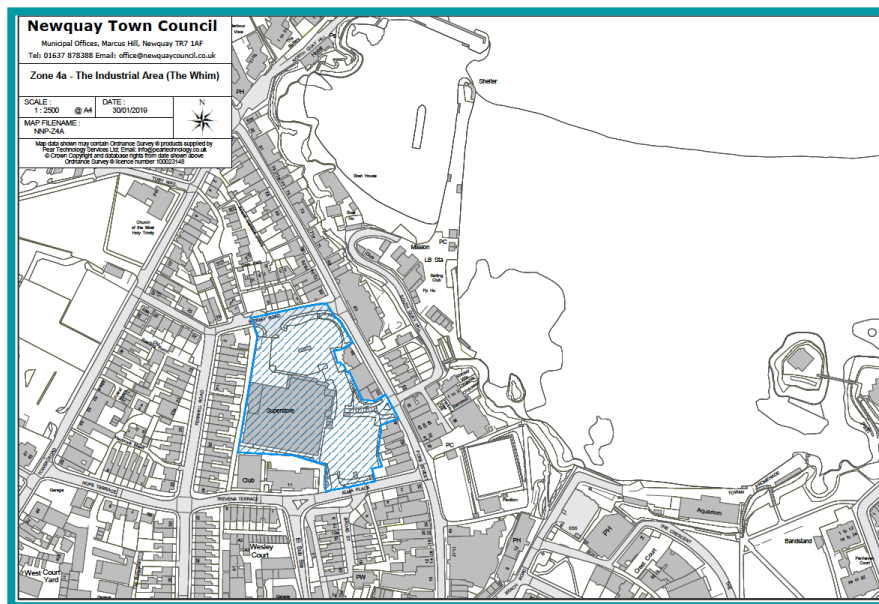


Erosion problems on the cliff face

4a: Industrial Area (The Whim)

General Description

This site has been demolished and rebuilt in the late 20th century to create a large supermarket, car parking and 3 smaller properties. It originally formed the end of the tramway, a Whim or winding engine and other properties creating a frontage onto Fore Street. While not retaining any historic features, in itself, it is an important alternative use in the central mixed-use area and has great potential to restore original character, scale, grain, and street enclosure in future redevelopment.



Key changes since CSUS study



- The supermarket had been built at the time of the CSUS study but minor improvements to the accessibility of the parking area have taken place.

Newquay Pattern Book

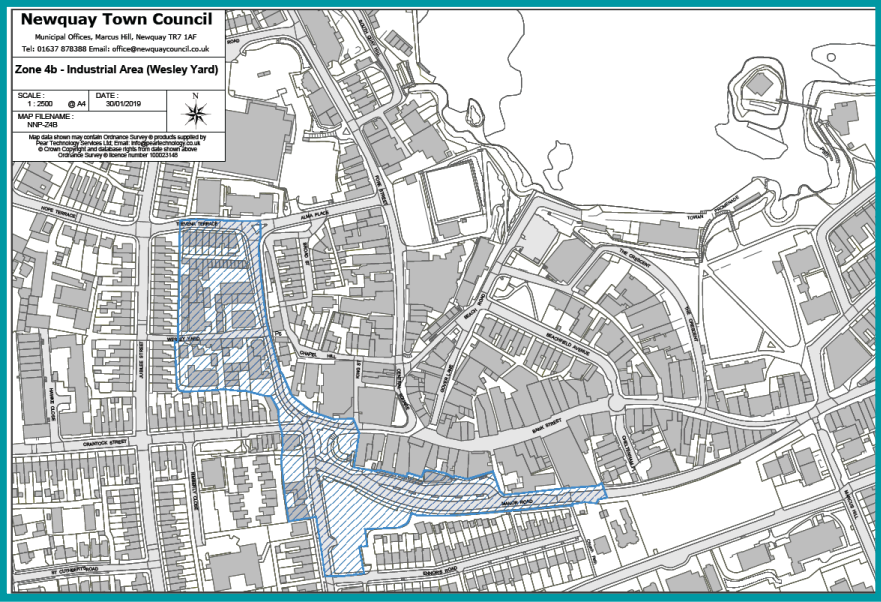
- Village High St p17 will highlight aspiration for reconstruction of the street


Important Characteristics of the Area

- See volunteer survey for more detail.
- Sits between 3 other character areas that are clearly part of the historic character of the town.
- Surrounding area is two storeys and would have had street frontage as the rest of Fore Street. Two smaller buildings are 1 storey.
- Good view from the raised car park towards the north coast, although not a public space. This may benefit an interim landscaping scheme.
- Sense of space retains some openness of previous use.

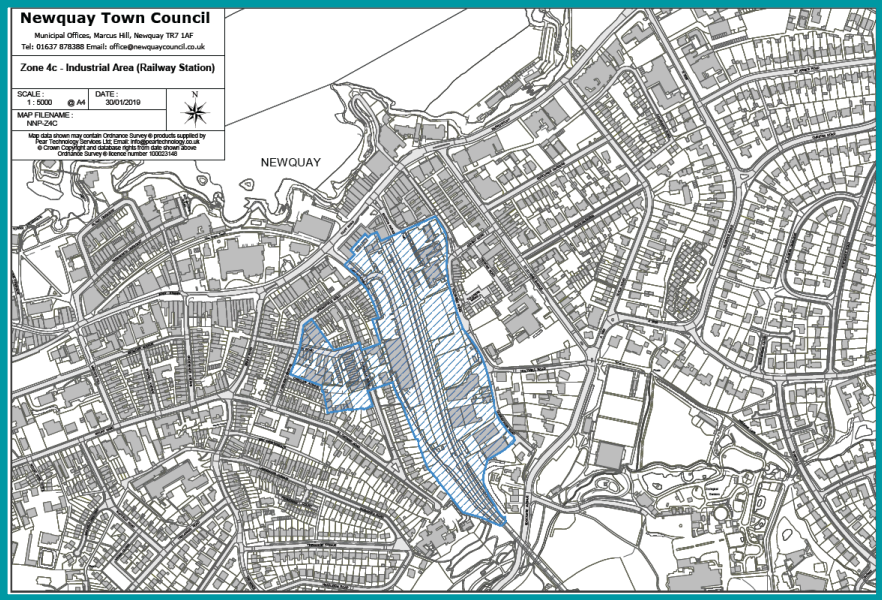

Key views to Protect	<ul style="list-style-type: none"> The view from the edge of the car park through the gap in the building frontage on Fore Street affords views of the North Coastline.  <p><i>Glimpsed views of North Coast from raised car park.</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> If this site is redeveloped, then some more enclosure and frontage on Fore Street would help reinforce this historic route although gaps to maintain glimpsed views of the north coast should be retained as part of the character. Ideally part of the site should always remain open to reflect the historic development of its distinctive past usage which linked the tramway to the port and a setting for the original whim.
Opportunities for Improvement	<ul style="list-style-type: none"> There is a great opportunity for interpretation on the site in a location where the view down to the sea is evident and to mark the line of tramway, purpose of the original whim and its relation to the harbour (with its tunnel link). Interpretation may also take in the origins of the Deerpark. Interim opportunities to improve the landscape scheme on the boundaries of the site would be worth considering but these should not stand in the way of a major redevelopment. Fore Street/Sidney Road corner is a primary location for some improvement to include vertical elements (trees) with opportunities for glimpsed views and potentially some seating.  <p><i>Open corner to Fore Street and Sydney Road</i></p>


4b: Industrial Area Wesley Yard

<p>General Description</p>	<p>Wesley Yard, Wesley Court and upper St George's Road area. This has retained a tight urban grain with surviving stone and brick built industrial structures with enclosed working yards. The yards and courts are usually enclosed within a larger residential block facing onto the main streets. These areas face onto smaller lanes and yards.</p> 
<p>Key changes since CSUS study</p>	<ul style="list-style-type: none"> Industrial buildings on St Georges Rd [Bullmores]opposite the car park, originally highlighted for sensitive adaptation, have been demolished and replaced with 4 storey mixed use block which changes the scale of the 2-storey street frontage. Small landscaped piazza has been created opposite the cinema on the edge of St Georges Road which has not yet met its potential to be a popular meeting place. The cinema is also new and although it is in area 1, it overlooks this area.
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> See Back Courts p26
<p>Important Characteristics of the Area</p>	<ul style="list-style-type: none"> See volunteer survey for more detail. Wesley Yard and Court present a distinctive network of backland courts, yards or lanes. Yard and Lane surfaces are often unmade. Comprised of small buildings, generally single storey or two storeys in height. Buildings flexible in use and primarily used for small commercial or industrial business close to the town centre.

Key views to Protect	<ul style="list-style-type: none"> The lower section of St George's Road opposite the car park affords views of town roovescape and the church tower.  <p><i>The entrance to Wesley Yard off Manor Road</i></p>
Key Designs to adopt in the Area	<ul style="list-style-type: none"> Even though individual uses may change over time, the unusual character of small courts and buildings with retained historic fabric is unique and distinctive. Small scale contemporary design can easily be integrated into conversions to develop a unique mixed-use quarter. Retain the layout and scale of buildings. Retain any historic fabric - it is an asset to future value.
Opportunities for Improvement	<ul style="list-style-type: none"> The main road junction (Crantock Street, St Georges Road, Manor Road) is poorly defined and dominated by the highway. Improvements could be made to the surfacing, crossing points, kerb lines and surrounding landscape to develop a much stronger sense of place to complement the adjoining landscaped open space. A careful management plan for the trees on the east side of Lower St Georges Road [pollarding] should be established to ensure these can be retained close to the new residential building and continue to enhance this street. Make tenants and owners aware of the value of the yards area and encourage retention of historic fabric and the diversity of businesses which are valuable to the town character. Consider signposting from the town centre as an alternative business area and including some interpretation on the site.

4c: Industrial Area - Land around the Railway Station

<p>General Description</p>	<p>Significant area of land identified for future regeneration. Includes 4 areas of different character and layout. Tolcarne Road, Railway station, Supermarket and Springfield Rd/Tor Road/Fairview Terrace.</p> 
<p>Key changes since CSUS study</p>	<ul style="list-style-type: none"> • The ASDA replacement of the original Co-op supermarket with minor improvements to car parking area. • Some improvements to the railway station spaces have been made but the anticipated regeneration scheme has not emerged.
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> • Historic train station characteristics p35 • Hotel and Station Forecourts p19 • Backcourts p26
<p>Important Characteristics of the Area</p>	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Generally comprising one or two storey height commercial buildings • Railway line creates a strong barrier between areas • West side more integrated into residential area.
<p>Key views to Protect</p>	<ul style="list-style-type: none"> • Most of this area has no views beyond its boundaries but a glimpsed view is noticed descending Tor Road over a bungalow and between buildings towards St Columb Minor and Quintrell Road in the  <p><i>View from tor Road</i></p>

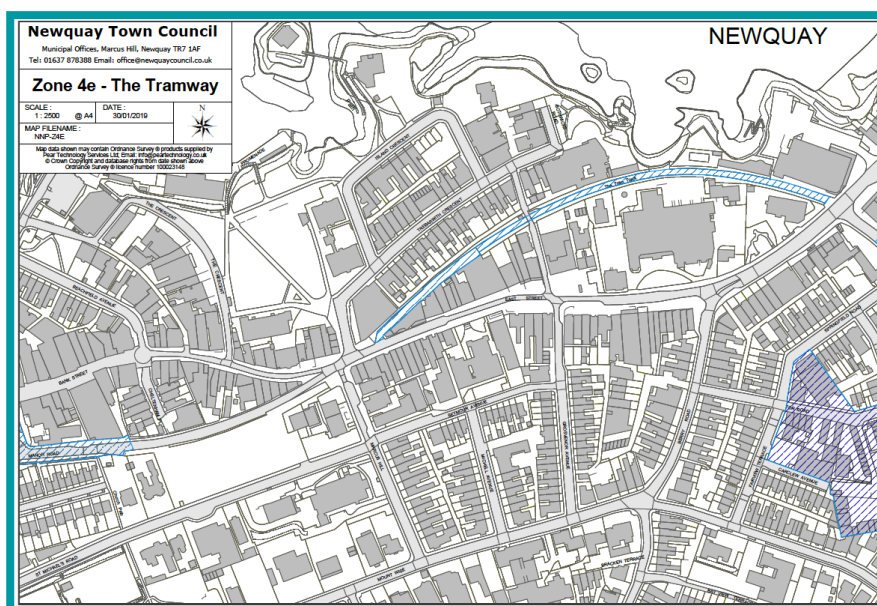
<p>Key Design to Principles to adopt in the Area</p>	<ul style="list-style-type: none"> • Principles for the development of the Cliff Road frontage buildings as part of an overall regeneration scheme are included in Section 6 below. • Development of a transport interchange park and ride, bus, rail • Development of a major public space at entrance to interchange with café/retail uses at this destination. • Incorporate multi storey car park for efficient use of land • Provide strong built frontage to the tramway route • Ensure any incidental green open spaces are usable and not retained as left-over space from highways. • Ensure any retail development is easy to access by pedestrians on safe accessible and attractive routes from the residential areas.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • A comprehensive regeneration of the railway station and adjoining land should be pursued again, with the development of a more detailed and achievable framework plan. This must include the 60's commercial development on the Cliff Road frontage and the small public space that is cluttered and has no sense of place. • Station Parade is no longer an inviting parade or entrance to the station and must ensure it retains active frontages and is an attractive street whether the regeneration is undertaken • Retain opportunities for small businesses close to residential area [such as Tor Road] rather than push out to out of town industrial parks. • Improvement for pedestrians to the supermarket should be considered, this is car dominated and disconnected from residential areas. <div data-bbox="552 1630 1114 2056">  </div> <p><i>The green area softens the harsh background of the police station [to the left of the picture] but is still left-over space which could be enhanced by better landscaping.</i></p>

4d: Industrial Area [incorporated into the survey for 10a]

4e: Industrial Area - The Tramway

General Description

This is the route of the original mineral tramway running in the expanded town centre. It has been retained as an important pedestrian link. Its western route follows the line of Manor Road as a two-lane street incorporating the replacement open bus station, to the site of the old Whim engine [now Sainsbury's]. There is a pedestrianised section at the eastern end enclosed by old hedgebanks and walls terminating the rear of properties along the route.



The sinuous route of the mineral tramway [4e] is shown connecting the industrial areas of the town [in blue] from the rail station in the east to Manor Road in the west.

Key changes since CSUS study

- The development of the bus station on Manor Road

Newquay Pattern Book

- N/A

Important Characteristics of the Area

- See volunteer survey for more detail.
- The pedestrianised section of the route is tranquil, and is a quieter alternative route, especially in the busy summer months.
- The special viewpoint in the pedestrianised section, overlooking Great Western Beach.
- The historic stone bridge on Bridge Road crossing the route is a key feature.

Key views to Protect

- Viewpoint partway along the pedestrianised section which opens up a dramatic view over Great Western Beach.
- Views from the bus station of the Church tower along Manor Road.
- The glimpsed view from the main East Street Manor Road/Trebarwith Crescent junction is very special, seen across the Killacourt.

Key views to Protect (cont.)	 <p><i>From the open viewing point along the tramway.</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Ensure the green character of the enclosing hedges of the pedestrianised section is retained and maintained. • Avoid any further development adjacent to the route that imposes large buildings and boundary walls that are not constructed in local stone. • Do not clutter the route by using for bin storage, or storage areas, or any projecting constructions. • Any painted walls could be enhanced by using a muted colour palette reflecting the colours in natural local stone. • The building at the apex [currently Fat Willy's] is a historic building and very important to the area. This could be subject to a restoration scheme incorporating a good shop frontage and presence in the longer view.
Opportunities for Improvement	<ul style="list-style-type: none"> • The character of the pedestrianised section of the tramway is being gradually eroded by development and activity on its edges. A "tramway code of practice" to enhance and protect this route could be created in co-operation with property owners. This could also discourage dumping of rubbish on the route. • A creative and sensitive repaving scheme with interpretation could make this very much more distinctive and valued destination in the town. • The area where the route crosses the busy Cliff Road junction onto Manor Road is cluttered and very confusing. This important junction in the town could be significantly improved with a simpler traffic calmed layout and de-cluttering. • Some consideration of how to discourage anti-social behaviour at the viewpoint along the route would be beneficial.

Opportunities for Improvement *(cont.)*



- The bus station is located at a convenient accessible location on Manor Road, a short distance from the main street, however its presence is completely understated for such an important function and it is not well overlooked. For 2018 an improvement scheme is proposed for the bus station and its facilities. However there remains an opportunity to undertake a more integrated regeneration scheme that could include more substantial landscaping, place making and potentially include the adjacent 2 storey block-shaped building on the western corner of the pedestrian route into the centre. This could better orientate visitors to the town centre on arrival. Continued provision of the toilet facilities at the site is considered important and requires a joined-up approach to deter anti-social behaviour in this space which is not well overlooked by surrounding buildings.
- Potential work to improve community safety and visibility at the entrance to the route from Bridge Road should be considered. This could be by lowering parts of the wall, installing complementary railings on top and or maintaining the foliage to an agreed height. In addition, there is potential for re-routing or screening of an exposed drainage pipe.



View from the tramway junction with Trebarwith Crescent

5: Secondary Commercial West

<p>General Description</p>	<p>Transitional area between the commercial core and the harbour related residential. Buildings are residential in scale but retain a high level of architectural detailing. Formerly a residential area but most buildings modified to accommodate shops and other commercial uses.</p>
<p>Key changes since CSUS study</p>	<p>The Cypress Fir referred to on page 39 of the study has now disappeared.</p>
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> • Mixed use buildings p32 – 34 • Village High Street p16
<p>Important Characteristics of the Area</p>	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Historic buildings are mostly retained but some elements hidden by later alterations • Rich architectural detailing evident in many buildings while earlier domestic cottages are more modest. • Some historic shopfronts and detailing remain, again sometimes hidden by later additions. • Fore street has more of smaller scale village centre character, but occasional grander larger scale buildings exist. This mix of architectural scale and detail and building types is an interesting feature • Many views of the sea and coast afforded on streets dropping down to the harbour. • Domestic scale and slightly wider street make it feel less enclosed than Bank Street • Predominantly two storeys in height but exceptions to three storeys.

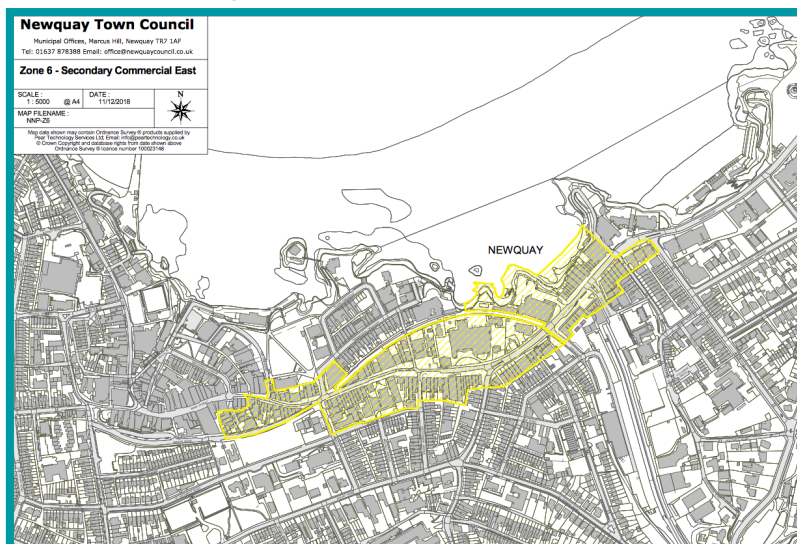
<p>Key views to Protect</p>	<ul style="list-style-type: none"> • From Fore Street overlooking the Bowling Green: across to north cliffs – and back to St Michaels Church and townscape • Sea views down Gover Lane and Beach Road • Glimpsed views down opeways and lanes <div data-bbox="564 421 1107 824">  </div> <div data-bbox="1136 421 1449 824">  </div> <p><i>View from Fore Street car park [left] and openway next to adjoining terrace [right]</i></p> <div data-bbox="564 931 938 1424">  </div> <div data-bbox="967 931 1340 1424">  </div> <p><i>View down the side of Fore Street car park [left] and South Quay Hill [right]</i></p>
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • Retain any historic fabric in buildings and shopfronts. Investigate to see what does remain and reveal this in any improvement or conversion scheme. • Shopfront design principles are set out in the Cornwall Shopfront Guide with additional references in the Newquay Shopfront Appendix. • New shopfronts and signing should be carefully considered in relation to the architectural design of the building façade. • The public realm and streets should not present any obstacles to free pedestrian movement, this will be a major problem for those who are less mobile. Street displays and A-boards can be an issue.

Key Design Principles to adopt in the area <i>(cont.)</i>	<ul style="list-style-type: none"> Any infill schemes for new buildings should respect the scale, form, richness in detail colour and material use of its neighbours to ensure it complements the character of the area.
Opportunities for Improvement	<ul style="list-style-type: none"> Understanding and application of the shopfront design guides will have a big impact on improving the quality and attractiveness of this area. Removing or redesigning some of the excessive street signage and street clutter could improve the environment. Key areas include the top of Beach Road. While good pavements exist along most of Fore Street some areas do present difficulty for accessibility and could be redesigned. Key areas include the South Quay Hill corners

6: Secondary Commercial Area East [East Street & Cliff Road]

General Description

Transitional area in historical expansion of the town centre, between large scale high density in the commercial core and smaller structures on the edge. Focused around the linear spine route dominated by retail. Level topography.



Area 6 Secondary commercial area East as shown in the CSUS study

Key changes since CSUS study


The major change is the development of the hotel and supermarket [Travelodge/Aldi] on the corner of the tramway and fronting onto Cliff Road


Newquay Pattern Book

- See page 19 for assessment of Hotel Victoria site and relationship to spaces.
- See page 31 and following for examples of important commercial buildings

Important Characteristics of the Area

- See volunteer survey for more detail.
- Area dominated by secondary commercial businesses and buildings.
- Good quality historic terraces
- High quality surviving architecture and special use of detail, decoration and quality materials evident but marred by many unsympathetic alterations to ground floor and signage.
- Important public realm focal points: in front of the Railway station, opposite the post office, East Street/Cliff Road junction and in front of the bus station.
- Focal buildings include the Victoria Hotel and the old Wesley Church.
- The cordyline palms outside the Post Office are a distinctive landscape feature in that area as are the row planted along the path on the Killacourt.

<p>Key Views to Protect</p>	<ul style="list-style-type: none"> • View down Trebarwith Crescent across Killacourt • View from Cliff Road past Aldi to the coastline and Atlantic Hotel • View from Cliff Road down the Tramway  <p><i>View of buildings along Cliff Road showing integrated design of long terrace.</i></p>
<p>Key Design Principles to adopt in the Area</p>	<ul style="list-style-type: none"> • It is essential that the development of buildings along the edges of the settlement that face the coast retain a locally distinctive character in the use of materials, scale, rhythm, architectural form and detail. This can be achieved with contemporary design as well as a more local vernacular. We need to ensure any further developments can help to reinforce that local distinctiveness. • The redevelopment of the station site should include the redevelopment of the buildings fronting Cliff Road. • Cliff Road frontage should be maximum of 3-storeys in height with a clear emphasis on the entrance to the station or transport exchange. • A public space could be incorporated as an entrance to the station and would ideally be enclosed on three sides by buildings giving maximum [retail/commercial] frontage to the space. However maximising sunlight into the space may favour a two-sided space with buildings on the North East and South East – also allowing views of the tramway. • Elsewhere in the area the priority should be to retain any historic structures and any historic detail retained on existing buildings. • In shopfront works investigate and reveal any surviving historic detail. • Ensure new shopfronts and signs complement the design of the existing building and restore original historic shopfronts if possible.

<p>Key Design Principles to adopt in the Area <i>(cont.)</i></p>	<ul style="list-style-type: none"> • Do not conceal any architectural detail with advertising signs of hoardings. • Important to ensure new development does not negatively impact on important views to the coastline or back to the town centre roovescape  <p><i>The impact of buildings on the cliff face overlooking Great Western Beach. Note the looming presence of the dark building which has a similar impact on Cliff Road.</i></p>
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • Redevelopment of the Cliff Road 60's development adjacent to the railway station and adjacent car parks presents the greatest opportunity in this area. • Promote the use of the Shopfront Design Guide and Appendix for Newquay to improve the retail environment. • Consider improvement projects to whole building facades where they may cover several ground floor shops to create a more integrated design. • The area at the foot of Marcus Hill [Bank Street East Street Manor Road junctions]is an incredibly busy, cluttered and confusing junction. It is a really important place in the town centre and deserving of a carefully thought out and consulted integrated public realm scheme. • The area in front of Newquay Railway Station and the station concourse is poorly designed and incorporates piecemeal improvements e.g. railings, canopy, tiling and seating with no overall design idea – a rethink would be beneficial to train passengers and all who pass through this area. • The tall black building that was developed on the site of the original Blue Lagoon club is out of scale with the surrounding townscape and detracting because of its dark materials, screened glazing and lack of relationship with the surrounding area. Any opportunity to redevelop this site or re-clad the building could bring significant improvements to the townscape and should be promoted.

**Opportunities for
Improvement** *(cont.)*

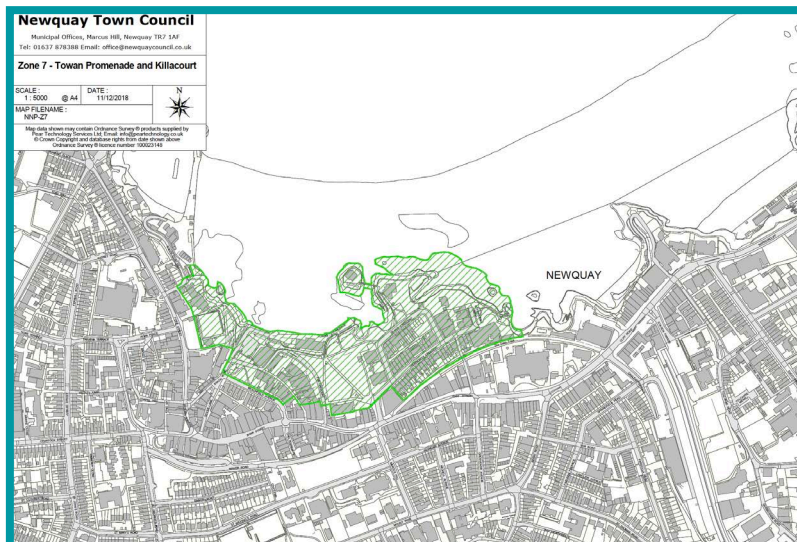


The Station entry and commercial frontage on Cliff Road

7: Towan Promenade and Killacourt

General Description

Victorian expansion of the resort taking advantage of clifftop location and proximity to commercial centre. Strong urban forms with crescents and the large open space of Killacourt. Overlooking sheltered Towan Beach and the iconic Jago's Island.



Area 7 Towan Promenade and Killacourt as shown in the CSUS study

Key changes since CSUS study

None identified

Newquay Pattern Book

- See page 31 and following for examples of important commercial buildings

Important Characteristics of the Area

- See volunteer survey for more detail.
- The Killacourt is a vitally important amenity space for the town, including the bandstand, a regular venue for outdoor events.
- The Killacourt affords wonderful views of Newquay Harbour, Towan Headland and The Atlantic Hotel, The Island as well as the coastline towards Trevoise lighthouse and no encroaching high building should be allowed to impede this vista.

Key Views to Protect

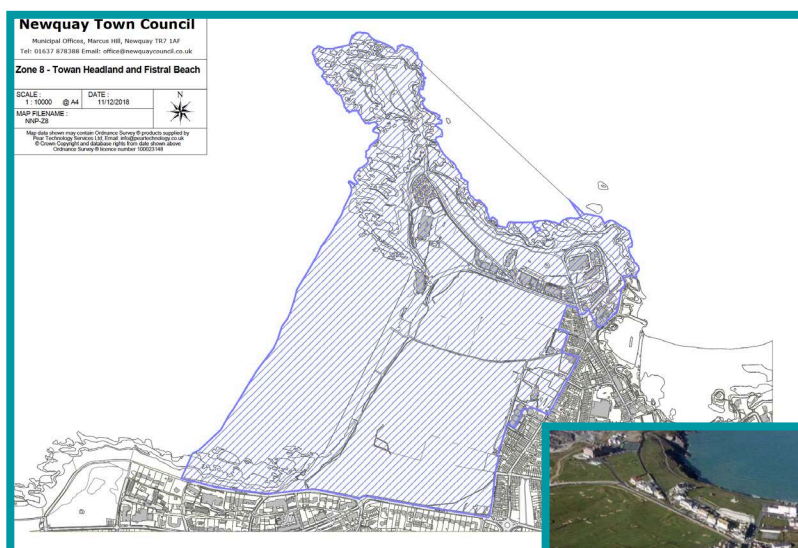
- The Killacourt affords wonderful views of Newquay Harbour, Towan Headland and The Atlantic Hotel, The Island as well as the coastline towards Trevoise lighthouse and no encroaching high building should be allowed to impede this vista.
- Many of the streets in this area afford special views of the sea that are framed by buildings on the street and coastline such as Gover Lane, Beach Road, Trebarwith Crescent, Island Crescent.
- Other routes running parallel to the coast edge have more open views such as The Crescent, Island Crescent.
- The best view is probably enjoyed from the viewing/seating area between the bowling green and car park and from that location of Fore Street

<p>Key Views to Protect (cont.)</p>	 <p><i>Iconic view from the Killacourt towards the Atlantic Hotel and Newquay Harbour</i></p>
<p>Key Design Principles to adopt in the Area</p>	<ul style="list-style-type: none"> • It is essential that the development of buildings along the edges of the settlement that face the coast retain a locally distinctive character in the use of materials, scale, rhythm, architectural form and detail. This can be achieved with contemporary design as well as a more local vernacular. We need to ensure any further developments can help to reinforce that local distinctiveness. • All new design proposals should be assessed for their visual impact on the coast and cliff edge when viewed from other points outside this area. • The characteristic scale of buildings and their rhythm and proportion of façade design is an important characteristic of the cliff edge townscape and should be retained unless there is a very special case made for an unusual setting. • No encroaching high buildings should be allowed to encroach on or impede the views and vistas identified.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • The bandstand in Killacourt is a vital asset to the town but could benefit from an improved design.

8: Towan Head and Fistral Beach

General Description

On the western edge of the town, this area includes most of the iconic landmarks amenities and coastal landscapes that help define Newquay. Designed on a grand scale with expanse of open coastal land, golf course, landmark hotels [Headland and Atlantic designed by Silvanus Trevail] and beaches. Headland Road is the prominent main street of hotels and apartments overlooking the northern edge of the open space. The area has many archaeological remains of iron age cist burials, Romana British settlements, tin mining, lead mining, military defences, old lifeboat house, the beacon, and early pilchard industry including the iconic Huers Hut and associated harbours.



Key changes since CSUS study

Many changes have occurred in this area since the 2003 survey, perhaps the most in the whole of the town.

- The Headland Hotel has been updated as a conference centre and hotel but with few external changes.
- Further expansion of the Headland Hotel associated holiday accommodation has been built to the north of the hotel, this now gives the appearance of a small settlement and has impacted on the setting of the headland.
- Headland Road has undergone the most changes, originally residential with some hotels this has been converted mostly to apartments with many original buildings demolished and rebuilt. See images below. "Westward Ho" a Silvanus Trevail building that featured in the CSUS report was demolished and replaced by Headland Point Apartments. Zinc has significantly impacted on the view of the Beacon/War Memorial highlighted in CSUS. See below. It has also impacted significantly on views from the war memorial open space to the south.

- Several developments have occurred around the Atlantic Hotel including flats and large residencies.
- The Surf Centre has been built – since affected by storm - and the new Lifeguards Centre on the north Fistril car Park.



Above: Headland Road in 2003

Below: Headland Road in 2018



One of the new apartment buildings “Zinc” has also impacted on the setting of the War Memorial when viewed along Fore Street – one of the main streets in the town that was highlighted in the CSUS study.



Left: View of the memorial in 2003 along Fore Street.



Right: View now compromised by apartment block.

Key changes since CSUS study (cont.)

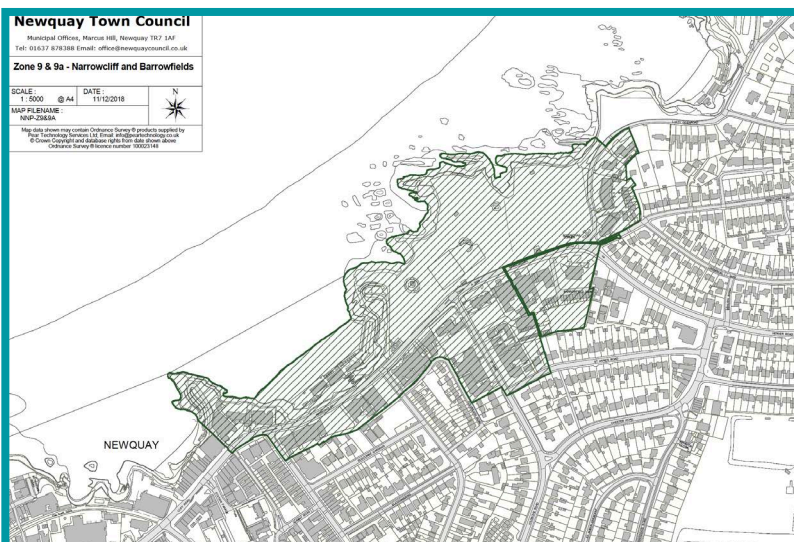
Newquay Pattern Book	<ul style="list-style-type: none"> Hotels p36 includes other large buildings P38 Buildings with verandas [includes Carmarth Hotel]
Important Characteristics of the Area	<ul style="list-style-type: none"> See volunteer survey for more detail. Key historic landmarks of the Headland Hotel, The Huer's Hut, The Beacon, The Atlantic Hotel – many now affected by tall apartment development and significant views compromised. The open space and golf course once “common land” a significant feature and focus of a development battle in the late 19th century. Headland Road – a significant and characteristic street and promenade, perhaps one surviving element of the masterplan for redeveloping the common in the late 1800's. The Atlantic Hotel and Headland Hotel were two buildings constructed from that plan. The Carnmarth Hotel and 12-14 and Fistrall Waves are the last surviving late Victorian/Edwardian buildings on that street and have some potential for restoration. The streets have a consistent building line, stone walled or hedge frontage and overlooks the whole open space to the south. Further increase of the development height along the street will impact on the balance of this prominent street elevation It is also important to allow the key landmarks beyond them to maintain prominence in the townscape such as the Beacon/ War Memorial and the Atlantic Hotel.
Key Views to Protect	<ul style="list-style-type: none"> Views of Headland Road across the golf course and open space. Views of the Headland Hotel across the golf course and open space From the Beacon and around the coastline to the Huer's Hut – spectacular views of Newquay Bay and beyond. Views from King Edwards Crescent across the harbour and to the town <div data-bbox="580 1585 1437 1946" data-label="Image"> </div> <p data-bbox="580 1957 1182 1995"><i>Newquay Bay was from King Edwards Crescent</i></p>

<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • It is essential that any significant developments in this area are tested at an early stage with the LVIA and photo montages because of the risk of unexpected impacts on its important landmarks, landscape setting and important views or vistas. • Maintain a consistent height and building line for development or extensions along Headland Road – now established at a raised ground floor [to allow for parking level] and between three to a maximum of five storeys above that floor level, with the top storey set back on the higher option. The Atlantic Hotel or Rocket corner development on higher ground should not be taken as a precedent. Any development proposals should also seek to maintain a longer view of the Beacon/memorial which is visible in the photograph above. • Developments should also keep below or align with the height to rear elevations of Headland Road buildings to the Beacon/War Memorial and its open space. Elevations should seek to create an attractive and active frontage to the north and good access and landscaping to the space.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • The Atlantic Hotel has been considerably modified in the 20th century and has lost most of its original architectural quality afforded by the architect Silvanus Trevail. The restoration of the building and harmonising of later additions could be a huge benefit to the town of this landmark and could be phased into improvement work in the future. • The car park area next to the Beacon/War Memorial could be significantly improved as part of a wider landscape scheme, to integrate with rear spaces and landscaping of Headland Road developments, Improve the gateway entrances to the car park and ensure a more sensitive setting for the open space and its memorial. <div data-bbox="577 1509 1469 1832" data-label="Image"> </div> <p><i>Carnmarth Hotel on Headland Road</i></p>

9, 9a: Narrowcliff and Barrowfields

General Description

Cliff-top development eastwards along Narrowcliff retained as important open grassland space enclosed definitive building line on two sides of larger apartments and some remaining villas to the east. Area now includes 9a which is development site behind the Narrowcliff frontage.



Area 9 Narrowcliff as shown in the CSUS study with 9a added.

View along Narrowcliff (Beresford Hotel) to show preferred maximum height of 4 storeys.

Key changes since CSUS study

Many hotel and villa sites have been redeveloped along Narrowcliff these continue to form the strong enclosure to Barrowfields. 'One Lusty Glaze' on the eastern side is however much larger in height than any other. The Rocklands apartment building is similarly out of scale and impacts badly on views of the town centre and historic skyline when viewing from the east.

Newquay Pattern Book

- The Pattern Book shows useful characteristics of buildings along the eastern section of Narrowcliff see p.20 with links to building types.
- See page 31 and following for examples of important commercial buildings

Important Characteristics of the Area

- See volunteer survey for more detail.
- The Barrowfields- a site of archaeological importance and retained grassland public space within the settlement. Outstanding location on cliff-top with unique coastal views

Important Characteristics of the Area <i>(cont.)</i>	<ul style="list-style-type: none"> • Enclosure of spaces such as Barrowfields and Narrowcliff cliff top street with buildings of similar height [4 storeys] and building line. • The vertical rhythm of bays and window openings is a characteristic feature even though style and detail may change between buildings. • Outstanding views of coastline sea and townscape maintained from streets and public spaces.
Key Views to Protect	<ul style="list-style-type: none"> • This area has many stunning views of the coast, sea, the open space of the Barrowfields and its surrounding buildings. This is seen from all points along Narrowcliff emerging from the two-sided street by Rocklands, and along Lusty Glaze Road. • Looking back to town there are key views of the roofscape and setting of the centre with the church tower and Atlantic Hotel as focal points. • The main streets joining Narrowcliff from the south have no focal point at their end, but the open sky is framed between buildings.
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • It is essential that the development of buildings along the edges of the settlement that face the coast retain a locally distinctive character in the use of materials, colour, scale, rhythm, architectural form and detail. This can be achieved with contemporary design as well as a more local vernacular. We need to ensure any further developments can help to reinforce that local distinctiveness. • Maintain a similar height of 4 storeys, and building line to create a strong consistent enclosure to major spaces and streets such as Narrowcliff • Avoid large scale use of strong colours for facades in this area. Use more muted natural shades, to fit in with the general background palette. E.g. warm grey, stone, cream, white. Use stronger colours only for detail emphasis. • Always test visual impact of proposals on longer views to consider scale, height, roofscape, colour. • Avoid horizontal emphasis in architectural composition if the building is fitting into a townscape/street frontage with vertical emphasis. • Ensure any development of backland sites are based on a well-connected network of streets. • Ensure change in scale from the buildings fronting Barrowfields to adjoining streets is retained in future developments. One Lusty Glaze should not be used as a precedent for new development.

<p>Key Design Principles to adopt in the area <i>(cont.)</i></p>	<ul style="list-style-type: none"> • Maintain well designed and landscaped forecourts to the main frontage buildings, locate car parking behind or under buildings if possible.  <p><i>View along Narrowcliff to show 4 storey height of buildings to be adhered to for any further new development, also note negative impact of large area of strong colour for facade.</i></p>
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • Future potential to repaint apartment block will help to harmonise the building into the streetscene. • The development of sites behind the Narrowcliff frontage should seek to integrate them properly into the townscape and its public street network. • The restoration and re-use of the Narrowcliff Hotel would have a positive impact on the streetscene and life of the town.

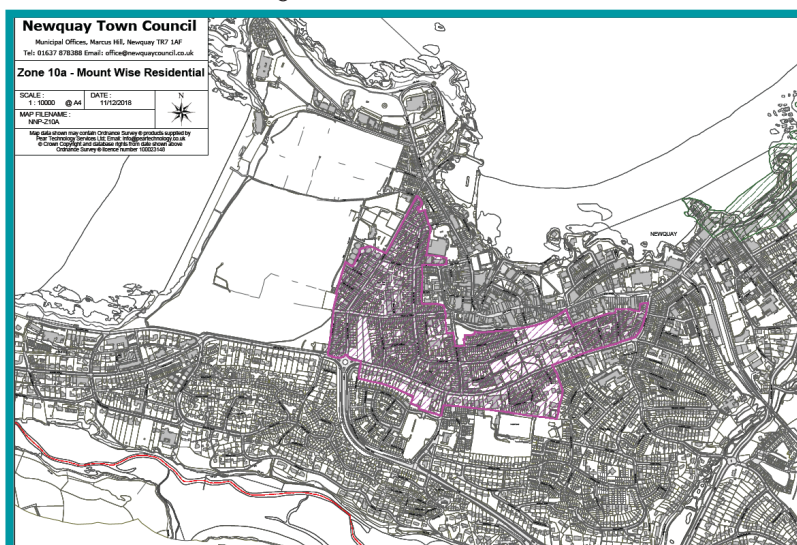
10a Mount Wise Residential and 4d Industrial Site

Predominantly loose grid layout of dense housing including close set terraces and a few larger villas. Small pockets of commercial space are intermixed and work satisfactorily. Many back lanes and pathways exist providing routes into the town centre.



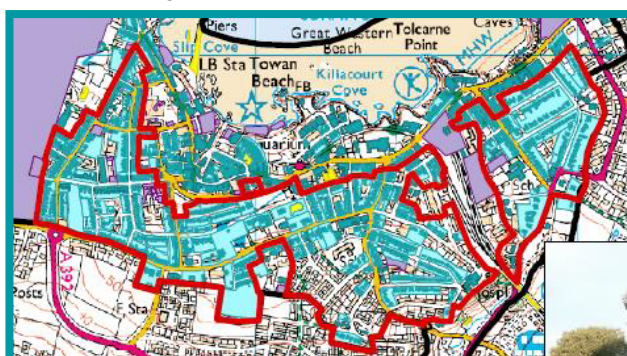
St John's Road leading to Jubilee Street & Fernhill Road

General Description




Green boundary above shows this study area

This study area includes part of CSUS area 10 [below left] and 4d [below right]



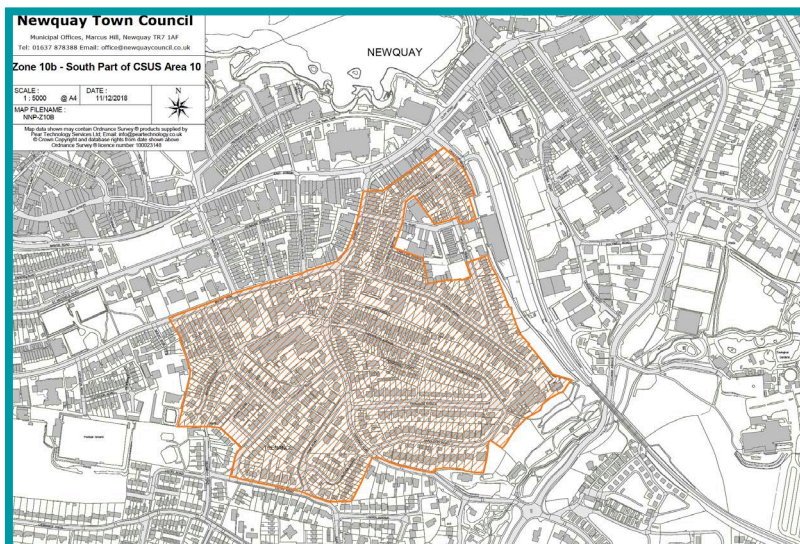
Key Changes since CSUS study	<p>Many new apartment blocks have been developed, the majority of these are out of scale with the surrounding buildings and have a negative impact on longer views. The main example is the Horizon Building at six storeys. The development at the corner of St Georges Road and Mount Wise is 4 storeys and also stands out due to the untypical form and use of materials.</p>
Newquay Pattern Book	<ul style="list-style-type: none"> • See house details and typologies p40-43 • Industrial building p46 • Residential Streets p21-22
Important Characteristics of the area	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Properties step up the sloping topography of Mount Wise creating distinctive stepped terraces retaining quality architectural detailing such as repeating bays. • Many tree lined terraces with strong forms and enclosure of the street. • Terraces often set back between small walled front gardens. • 2-3 storey height with attic rooms. • Mount Wise Gardens is an important public space with dramatic views of the townscape, Newquay Bay and the north coastline
Key Views to protect	<ul style="list-style-type: none"> • The view from Mount Wise Gardens – particularly noting the Atlantic Hotel and even the war memorial/beacon above the roofscape • The view along Mount Wise towards the church with tall residential buildings <div data-bbox="577 1404 1222 1767" data-label="Image"> </div> <p data-bbox="1230 1742 1495 1776"><i>Mount Wise Gardens</i></p> <div data-bbox="577 1803 1222 2123" data-label="Image"> </div> <p data-bbox="1230 2022 1495 2134"><i>View along Mount Wise towards the Church.</i></p>

Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Do not build over the typical height of surrounding buildings which is two or three stories with attic rooms depending on the street. • Ensure any infill development on the main streets retains the scale and rhythm of the street frontage, street enclosure, building lines and roofscape. • Encourage the retention of mixed uses in the area to help develop local micro businesses. Pay special attention to avoid conflicts over noise, parking, in mixed use areas. Ensure other uses buildings reinforce the enclosure and street building lines and local materials where appropriate. • Avoid the loss of any historic fabric in new schemes. Whole buildings, surfacing materials, incidental outbuildings and particularly walls should be retained and incorporating them into the design.
Opportunities for Improvement	<ul style="list-style-type: none"> • Large unused commercial site (old Jewson's site) presents a good opportunity for integrating new commercial, light industrial or residential into the area. Major opportunity to retain and convert the distinctive stone building. The old knitting factory in Crantock Street is a good example of a conversion scheme. • There are many opportunities to improve (often unadopted) back lanes which suffer from poor surfacing and dumping rubbish. These could have improved lighting especially where used as pedestrian routes into the town. <div data-bbox="584 1245 927 1697">  </div> <p><i>Historic stone pumping house in area 4d with potential for conversion as part of wider regeneration scheme.</i></p>

10b: South East part of CSUS area 10

General Description

Housing area focusing on the spine of Trenance Road running north south. Located on a ridge with areas to the north looking out to sea views and areas south looking out to Trenance Valley and the countryside. This is an area of connected streets with housing development comprising of late Victorian and Edwardian terraces, 1930's private housing including bungalows, and some council housing, mostly post 1930 semi-detached houses. Trenance Learning Academy and a Children's Centre are at the heart of this area.



Key Changes since CSUS Study

Nothing mentioned specifically in the original CSUS text about this part of character area 10.

Newquay Pattern Book



- Town residential p21
- Resort Residential p22

Important Characteristics of the Area


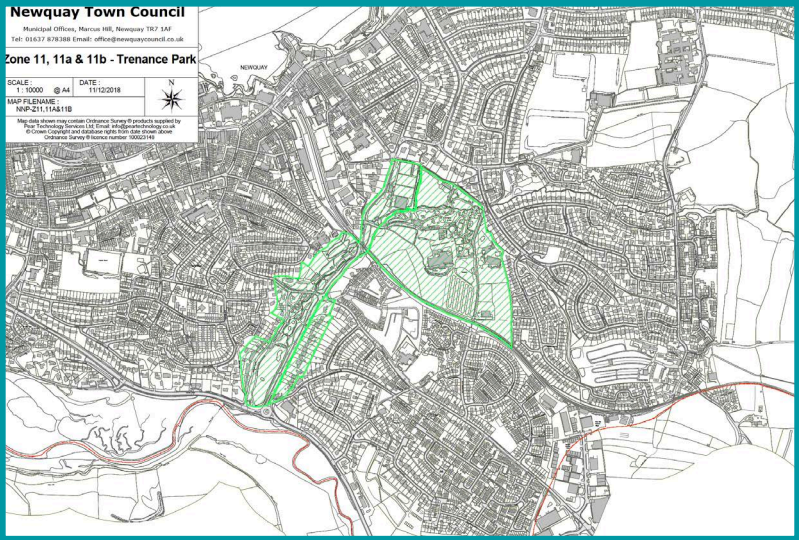
- Loose gridded network of streets with perimeter blocks of housing, few cul de sacs. Secondary streets run along the contours, main streets run up and down the hillside.
- All houses facing the street, with enclosed front gardens and back to back rear gardens.
- Street trees in verges and planting in gardens adds to local character.
- Earlier terraces in core area, often with 2 storey bays and dormer windows, good materials and details, mixing brickwork and stone.




Left: High quality bay fronted 19th century terraces on Berry Road.

<p>Key views to protect</p>	<ul style="list-style-type: none"> • North view along Mayfield Road frames the church tower • Trenance Road has narrow views of the sea looking north. • Trenance Road and other south facing streets give views of countryside looking south. • Bay View Terrace – the public street has limited views today over a wall and low development but could be affected by taller development.  <p><i>Trenance Road looking North</i></p>  <p><i>Trenance Road looking South</i></p>
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • Retain heights of neighbouring houses on the street – generally one or two stories with attic rooms or dormers. • Maintain the building line and walled enclosure on front gardens, smaller gardens in the denser core area of Victorian and Edwardian housing. • Ensure any new development provides adequate off-street parking to reduce congestion.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • Measure to reduce street blockages due to parked cars on Agar Road, Rawley Lane, Hawkins Road affecting bus route, especially at school pick up time e.g. double yellow lines as key points. • Install more bins including dog bins in well used locations. • Maintain green spaces and grass verges which are an important part of the local character – discourage parking. • Seek to enhance or provide seating and landscaping in a tranquil open space to take advantage of the views.

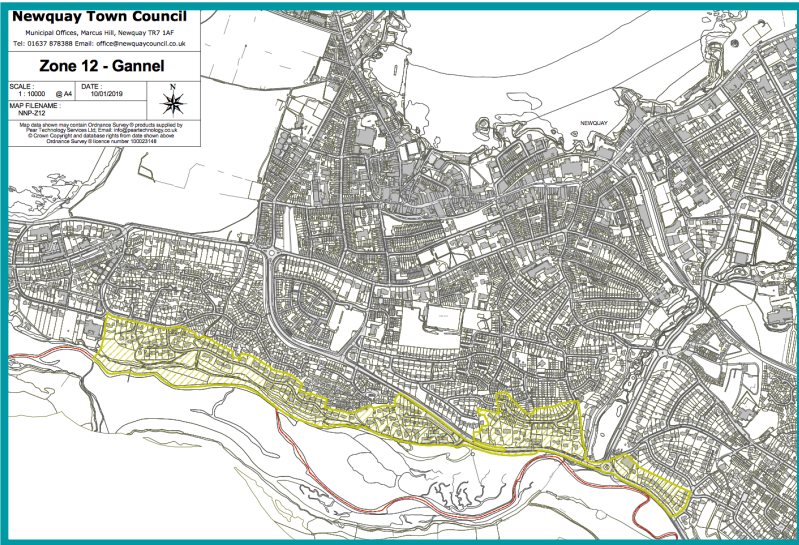
11: Trenance Park *Now including Trevemper Road as part of its setting*



<p>General Description</p>	<p>This valley forms a setting for a sub-tropical park with boating lake, gardens and sports facilities. This valley is crossed by the railway viaduct which is a key landmark in many views.</p>  
<p>Key Changes since CSUS Study</p>	<ul style="list-style-type: none"> • One contemporary house built on south side replacing older property • Three willow sculptures by Serena D' La Hay have been removed after falling into disrepair.
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> • Parks and leisure p24 • See page 55 Walls and Boundaries
<p>Important Characteristics of the area</p>	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Gardens with sense of space and tranquillity [1906] • Focus for Leisure and sporting activities • 1874 listed viaduct prominent feature in all views • Water feature including ponds and seasonal boating lake [created 1930's] • Supporting facilities in buildings restaurant tea rooms café kiosk

Important Characteristics of the area <i>(cont.)</i>	<ul style="list-style-type: none"> • The listed Heritage Cottages with its recent restoration, museum and active Friends Trust. • The trees and landscape in the gardens of surrounding development which blend into the green landscape of the park. • Overlooked by surrounding buildings fronting onto roads that edge the space. • Three distinct areas with different character and activities.
Key views to Protect	<ul style="list-style-type: none"> • The views outwards from the park to the east and west which include the development and gardens on its adjoining sides. • Views from the southern gardens look over the lake across the valley of the Gannel Estuary. This should be kept clear of any other development. • Views along Edgcumbe Avenue south towards the viaduct with some housing on one side and leisure areas on the other. • The views along Trevemper Road and Trenance Lane which include the park but also include the front boundaries, landscape and elevations of adjoining properties • The views at the main junction of Trenance, Trevemper and Treninnick essentially are of four green corners. The residential boundary trees.  <p><i>View across the lake and the Gannel Estuary beyond</i></p>
Key Design to adopt in the area	<ul style="list-style-type: none"> • Retain the scale of development around the fringes of the park, i.e. one and two storeys detached, or semi-detached houses set within gardens are typical and strongly characteristic on Trevemper Road. While this road is technically outside of the CSUS area it is an essential part of its character and setting and so is described here. • On Trenance Lane some 3 storey frontages in split level development have been introduced. Any further development of this height should aim to reduce the footprint of blocks to the more typical lengths of detached or small terraces and must retain a green stone hedge on the frontage to retain the special character of this lane.

<p>Key Design to adopt in the area <i>(cont.)</i></p>	<ul style="list-style-type: none"> • Cornish slate is very typical of the surrounding buildings and would be important to retain or reintroduce wherever possible. • Retain the garden landscape as much as possible to retain the green setting around the park. • Ensure the scale, height and character of the old gardener's house [now private] in the north west corner is not lost or altered in such a way to affect the setting of the park. • All sections of the park above and below the viaduct should be retained with an open green edge and any additional buildings/ or structures should be designed as attractive pavilions within the open green space.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • Consider opportunities for voluntary work to support dwindling finance that will be available from Cornwall Council. See the model of The Friends of Trenance Cottages or other Friends of Woodlands or Parks in the county. <div data-bbox="587 857 1168 1245">  </div> <p><i>A thoughtful, individual and well-designed contemporary home. Although untypical in its lack of a pitched roof, it retains the scale of individual houses on the road.</i></p>

12: Gannel Side

<p>General Description</p>	<p>Adjacent to the Gannel Estuary with its SSSI designation from Trevemper Bridge to the Pentire headland.</p> 
<p>Important Characteristics of the area</p>	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Predominantly one or two storeys late 20th century development. Bungalows with dormers. Occasional 3 storey flats. No real distinctive character in architectural quality – typical mixed style suburban developments. • Typically, large detached properties on medium sized plots • Slate roofs and stone walling help in setting. • South facing views over the estuary. • Landscape is not that significant in gardens in Trevean Way. • Larger gardens noticeable in Tregunnel Hill and large number of [modern] Cornish hedges are significant. • Apartment development on the Old Gannel Road looks out of scale and urban in character – quite different to any other development. • Tregunnel House, its surrounding garden spaces, the green lane, trees and stone structures are important historic and landscape features in the heart of this area.
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> • See page 55 Walls and Boundaries
<p>Key Views to protect</p>	<ul style="list-style-type: none"> • Trevean Green: the retained green spaces on the south side of Trevean Way maintain special views across the Gannel, one of the routes of the South West Coastal Path • The car park on Tregunnel Hill presents good views across the estuary.

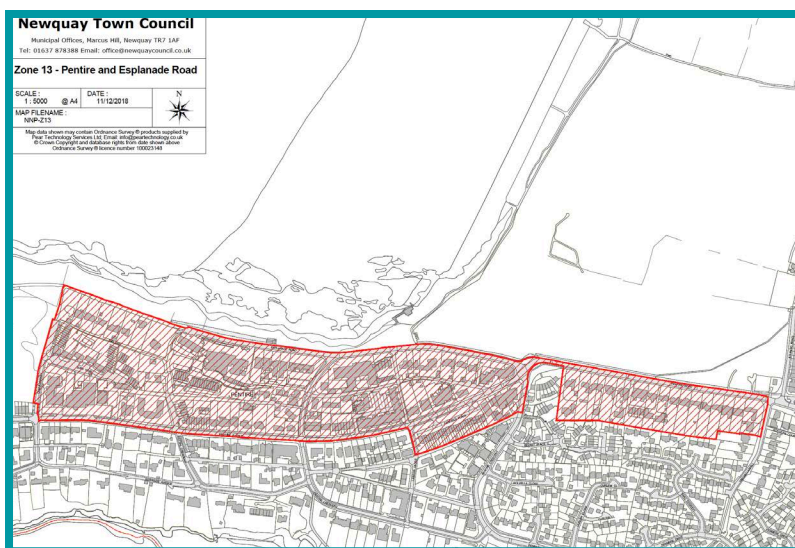
<p>Key Views to protect <i>(cont.)</i></p>	<ul style="list-style-type: none"> • Tregunnel House and its setting. • Views of the Gannelside when seen from the South West Coastal Path across the estuary or in the estuary are very important  <p><i>View from Trvean Green towards Penpol Creek & the South West coastal path.</i></p>  <p><i>View from The Gannel Car Park</i></p>
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • Any replacement or infill development should be of similar scale, form and height as the surrounding development. • New development, including significant extensions, should also maintain good spaces around buildings • Garden planting is important and should always be part of any development scheme to retain to help the buildings sit within this special landscaped setting. • The setting of the historic farmhouse must be protected as well as its surrounding landscape, this includes the field to the north of the house. Note that the field immediately north of Tregunnel House incorporates a main sewer serving nearby development. • The green spaces of Trevean Green should be retained • Encourage continued use of Cornish hedges or stone faced planted banks as landscape features on property boundaries particularly along streets. These will help the developments setting in the hillside. Avoid the use of concrete blockwork or rendered walls for boundaries. • Encourage the use of natural slate in roofing or re-roofing projects as these become more prominent on hillsides. • Encourage the use of more muted natural colours for render painting to gradually soften the appearance of this development on the hillside.

Opportunities for Improvement	<ul style="list-style-type: none">• Promote the above design principles re garden planting, use of slate, colour and Cornish hedges as a way residents improvement projects can gradually improve the appearance of this important development edge.
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13: Pentire and Esplanade Roads

General Description

Residential area incorporating many apartment blocks and 3 hotels, overlooking Fistral Bay to the north




Newquay Pattern Book

- See page 55 Walls and Boundaries

Important Characteristics of the area

- See volunteer survey for more detail.
- Several unmade green lanes running east west between main roads.
- Most buildings set back from the pavement behind gardens or parking areas
- Esplanade Green, a formally designated Village Green, is a special public space located immediately on the west side of this area used for walking.
- To the north the Golf Links are also a vital green space
- Pentire Road [with a couple of 3 storey medium scale apartment buildings] retains individual two storey houses set back behind landscaped gardens and continuous low walls – as a frontage to the open space. It also retains most buildings with slated pitched roofs.
- Esplanade Road [and Pentire Ave to the rear] has changed to incorporate many three and sometimes four storey large apartment blocks. The curved roof shapes look incongruous in this setting although many buildings have flat roofs.
- The junction of Pentire Road, Esplanade Road and Pentire Avenue is important in the townscape and has three purpose designed buildings to mark these corners.
- Landscaping in front gardens and a stone front boundary wall are important features that ensure a good relationship between building plots and the open landscape.

<p>Key Views to Protect <i>(cont.)</i></p>	<ul style="list-style-type: none"> • Special views out from Esplanade Road and Pentire Road across the beach, bay, golf links, Headland Hotel to the north and The Green to the west • The view north emerging from both ends of Pentire Crescent, particularly frame this northern view. • Along Pentire Avenue looking east 
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • The height of proposed new buildings should not exceed the height of adjoining buildings in this area. Generally, this should be restricted to 3 storeys. • The redevelopment of the Fistral Bay Hotel site is vital to the improve the quality and attractiveness of this area especially as it is located on a very prominent site. A development of no more than 4 storeys is appropriate at this location to maintain the general scale of the surrounding buildings. It should also be designed to “turn the corner” well at its apex, and present good frontages with active ground floors to the streets on both sides. • Building should be set back behind a front garden or landscaped paved space that is enclosed by a stone boundary wall or Cornish hedge. Corner buildings at entrances to courts or buildings with opes or courtyard developments can be brought closer to the pavement to form a stronger enclosure. • Any new development along Pentire Road should reflect the smaller scale of buildings, typical pitched roof form and encasing garden boundary walls.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • The establishment of some well-informed good practice guidance on the use, particularly detailing and finishing of materials would help the weathering and long-term maintenance of buildings in this exposed maritime climate. Many of the more recent apartments are already showing considerable problems. One exception is the more traditional build on the Pentire Rd/ Pentire Crescent corner.

Opportunities for Improvement *(cont.)*

- Improvements should be considered to the pedestrian movement across the Pentire Road/Pentire Crescent/Esplanade Road junction which is particularly difficult in the busy summer season and even more difficult for elderly or people with disabilities. This could take the form of a contrasting surface, table top junction, incorporating refuges or the installation of controlled crossings.



Pentire Avenue Looking East



House 1 with Cornish Stone Hedge



HOUSE 2
Modernised with Cornish Hedge Banks
Narrow unmade lane beside

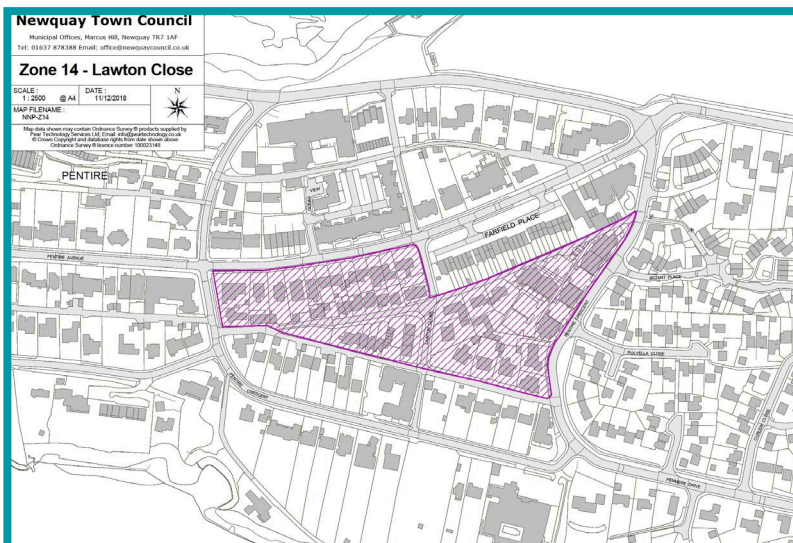


View Across Golf Links

14: Lawton Close

General Description

Infilled development with frontage onto Pentire Ave and Pentire Crescent. Part detached and semi-detached houses of two storeys, part new development of 4 storey apartment block.



Important Characteristics of the area


- See volunteer survey for more detail.
- Interesting backlane development in the triangle edged by Lawton Close incorporating small but well used open space, a formally designated Village Green
- Typical large houses on Pentire Avenue.
- The large Tre Lowen apartments dominate area due to their untypical height of 4 storeys and lack of local distinctiveness in the design.

Key Views to Protect

- Glimpsed view down Lawton Close
- Views of the coast from the western Pentire Avenue/ Pentire Crescent junction



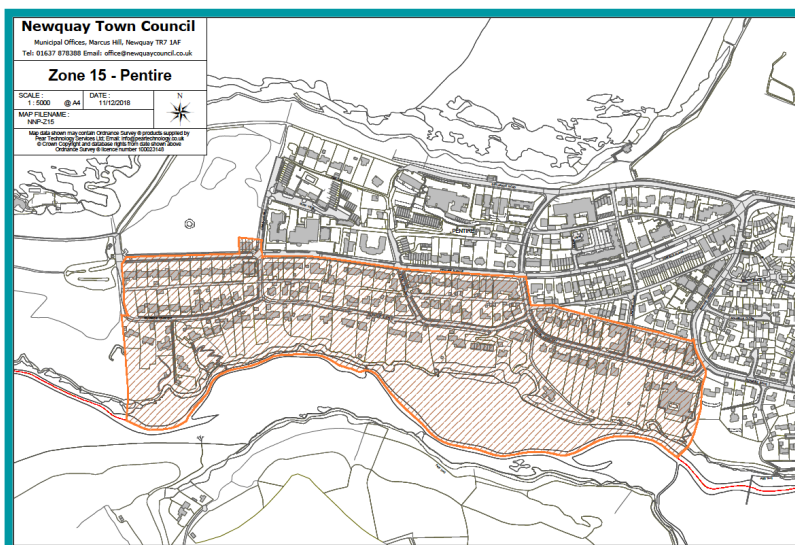
The village green left and view from Lawton Close right.

Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Few opportunities for new build but if opportunities arise they should reflect the height of the whole neighbourhood at 2-storeys. • Opportunities to develop stone walls on boundaries to the public areas and additional landscaping would enhance the area.
Opportunities for Improvement	<ul style="list-style-type: none"> • Enhancements to the back-lane development boundaries and the small village green itself could be further enhanced to make the area more distinctive and more of an asset to the community, however these unmade roads are likely to be in joint private ownership or none. • Improvements to the surfacing and potholes in the unmade lanes would be of benefit to all. <div data-bbox="587 703 1007 1258">  </div> <p data-bbox="1018 1189 1342 1258"><i>View from the Pentire Crescent/Avenue junction</i></p>

15: Pentire - Gannel Facing

General Description

Residential area bounded by the Pentire headland and the River Gannel. Overlooking special environment of Gannel Estuary and SSSI. Effectively set on both sides of single east west street following the contour. The development on the south side of this route backs on to the estuary. There is no public route along this northern edge of the estuary. Development started with a few large detached houses in the 1900's and the majority of the development followed in the 1960's.






Important Characteristics of the area

- See volunteer survey for more detail.
- Low buildings on the south side of Riverside Crescent, Riverside Avenue, and Pentire Crescent allow views over as well as between properties from the public street to the estuary.
- Mix of architectural styles. Mainly two storey houses or bungalows with dormer extensions.
- Corisande Manor is one of exceptions built 1895. It has 4 stories on the Gannel side with tower.

Key Views to Protect

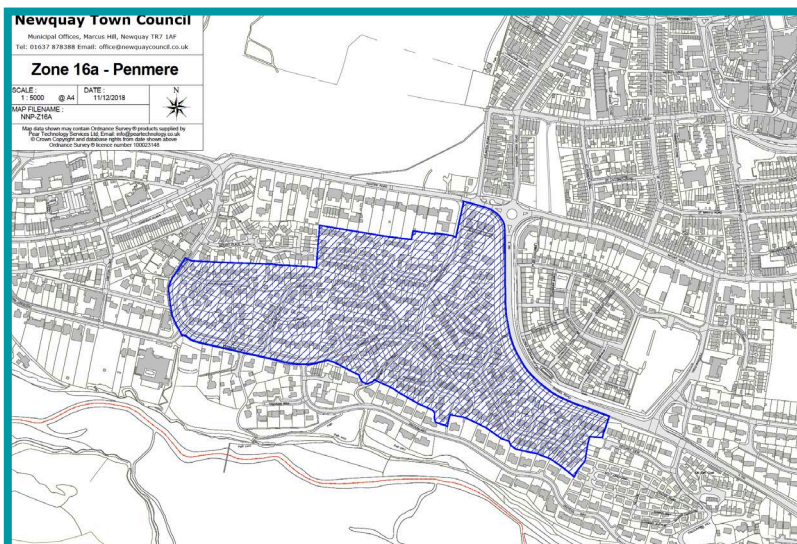
- Views of the Gannelside when seen from across the estuary or in the estuary are very important, especially of the steep deep gardens and cliff edge.
- Views along Riverside Crescent over the roofs of the buildings on the south side
- The unadopted lane section of Riverside Avenue, route of the South West Coastal Path
- Views from widened drive location on Riverside Ave.

<p>Key Views to Protect <i>(cont.)</i></p>	 <p><i>Corisande Manor</i></p>  <p><i>Steep landscaped gardens above the cliff</i></p>  <p><i>Riverside Avenue</i></p>
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • Avoid further development of buildings and substantial built structures within the cliffside gardens facing the estuary. This green steeply landscaped area is vitally important to the setting of the estuary and the edge of the settlement. This can also be affected in gradual increments by a build-up of garden structures • Encourage continued use of Cornish hedges or stone faced planted banks as landscape features on property boundaries particularly along streets. These will help the developments setting in the hillside. Avoid the use of concrete blockwork or rendered walls for boundaries. • Encourage the use of natural slate in roofing or re-roofing projects as these become more prominent on hillsides • Encourage the use of more muted natural colours for render painting to gradually soften the appearance of this development on the hillside
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • Promote the above design principles re garden planting, use of slate, colour and Cornish hedges as a way that residents projects can gradually improve the appearance of this important development edge. • Promote dark sky principle to reduce illumination on land around both sides of the estuary

16a: Penmere

General Description

A quiet residential area based mainly on detached two storey houses in cul-de-sacs. There are several narrow pedestrian cut-throughs used, to access other cul-de-sacs. Landscaping is generally missing in the estate and the estate seems dominated by tarmac and yellow lines between buildings. The central area feels closed in but with little character to the way the streets are enclosed.



Important Characteristics of the area

- See volunteer survey for more detail.
- A quiet residential area based on cul-de-sacs. It is typical of many other late 20th century estate developments around the country and apart from some longer views of landscape to the south has no real locally distinctive features.
- Two storey housing, set close together but little formal grouping.

Key Views to Protect

- Some glimpsed views down streets running north south such as Curlew Close give views of the landscape across the Gannel to Crantock parish.



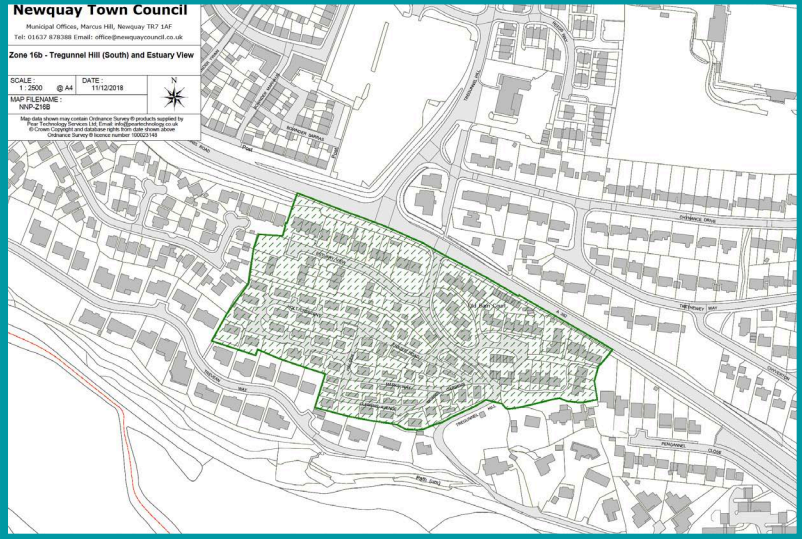


View down Curlew Close


Key Design Principles to adopt in the area

- There is little opportunity for new development
- Potential for improvement could focus on redefining the streets creating shared space or home zones with better landscaping and slower traffic speeds.

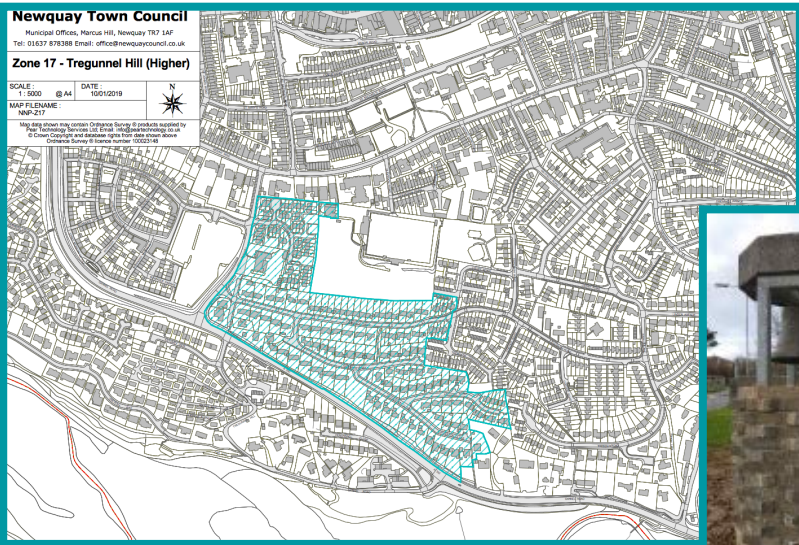


Opportunities for Improvement	<p>See Design Principles above</p> <p>There is no open space or play area in the area. Trevean Green and the River Gannel is accessible via the cut-throughs to the South and the play area at Atlantic road, which is scheduled to be improved and enlarged, is just to the North of the area and can be reached via Trethellan Hill - but that is not easily walkable from the further reaches of the estate. So, any future opportunities to create better access or signage to this or any facilities in nearby areas would be an advantage.</p>
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16b: Tregunnel Hill [South] and Estuary View

<p>General Description</p>	<p>Late 20th century housing built to the north of a historic farmhouse. Primarily a development of single storey permanent mobile home retirement complex. The north side also incorporates two developments of 2 storey semi-detached and terraced housing.</p> 
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> • See p 55 walls and boundaries • P57 and following Plants and Landscaping
<p>Important Characteristics of the area</p>	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Designs generally laid out to permit as many views of the estuary as possible. • Tregunnel Hill garages concealed behind bank and stone wall. • Cul-de-sac development centred on Tregunnel Hill [south]
<p>Key Views to Protect</p>	<ul style="list-style-type: none"> • Housing on south side of Estuary View is orientated north south to allow views of the hillsides beyond the Gannel Estuary. • Tregunnel Hill was also laid out to permit views from south facing properties and small green areas over lower garage blocks, and of green wooded area at the bottom of the hill.  <p><i>Above & Below: The open space with a view on Tregunnel Hill</i></p> 

Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • No further building on green spaces • Single storey development in Tregunnel Park should be retained to allow views over from higher ground • Opportunity to improve side and rear boundaries backing onto Tregunnel Hill by creating a tall Cornish hedge or stone wall could be considered. The hedge could include a temporary fence behind until the planting becomes established.
Opportunities for Improvement	<ul style="list-style-type: none"> • Enhancements to the greenspace with the post box could make the area more convivial and usable to residents, perhaps with the installation of a bench and some sensitive planting. The character of the space and pedestrian safety could be enhanced by changing the surfacing materials of the street at this point to give warning of a more pedestrian area. The restoration and revealing of the old barn and walls below this green space would help to make this lower area more distinctive. <div data-bbox="582 808 1214 1281">  </div> <p><i>The Old Barn Tregunnel Hill</i></p>

17: Tregunnel Hill [Higher]

General Description	<p>Primarily 1960's cul-de-sac residential development of dormer and split-level bungalows. On south facing slope overlooking the Gannel Estuary. Denser 70's/80's 2 storey development around Reeds Way</p> <div data-bbox="563 383 1366 925">  </div> <div data-bbox="1262 584 1465 981">  </div> <p><i>Above right: structure with bell outside the fire station</i></p>
Important Characteristics of the area	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Generally spacious layout with views to the south, except for Reeds Way • Low Cornish stone walls in front gardens are a strong feature • Fire station on Tregunnel Hill
Key Views to Protect	<ul style="list-style-type: none"> • Due to layout, topography and low height of properties most buildings enjoy some view across the estuary • Viewing point from green space at top of Chyverton Close across estuary. • Good views down Tregunnel Hill. <div data-bbox="587 1628 1187 1966">  </div> <p><i>Cornish stone garden walls</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Retain lower building heights • Enhance garden frontages on main streets with stone walls and planting.

Opportunities for Improvement

Harsher street environment around Reeds Way could benefit from new public realm design incorporating Home Zones and greening of environment.



Dense environment around Reeds Way does not have the same sense of space and views.

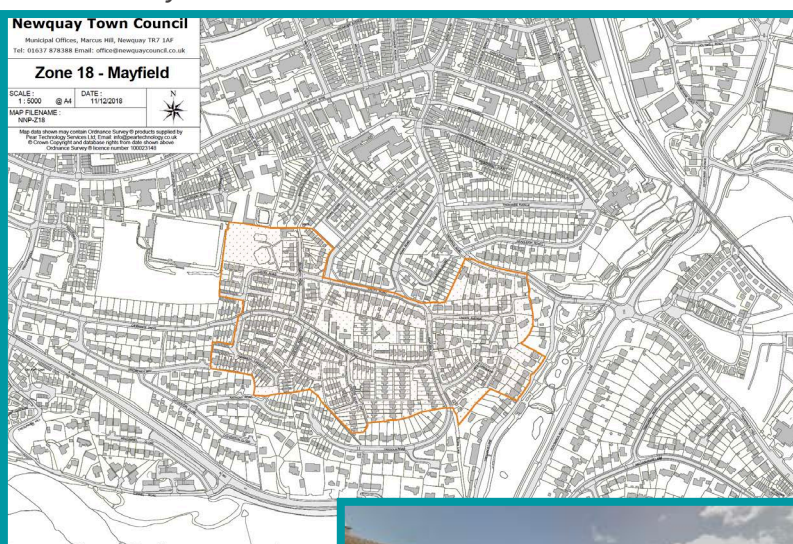


Typical dormer bungalows on large plots on Chynance Drive

18: Mayfield

General Description

The area comprises mainly of post war council housing [many prefabricated panel construction] including a group of 3 storey flats in open space. Other housing includes the distinctive 60's monopitch roof terraces in Hawkins Road and Chynance Drive and another block of three storey flats. This is a neighbourhood with open space and many views to the south. It includes a recreation ground with play area off Listry Road.





Mayfield area boundary in orange.



Chynance Drive/Hawkins Road runs through the heart of the neighbourhood

Important Characteristics of the area

- See volunteer survey for more detail.
- Very distinctive post war social housing of varying design and periods including panel construction.
- Good size recreation space and play area
- Spacious street layout with verges
- Street terracing down towards the Trenance and Gannel valleys
- Good enclosed front gardens in central area
- Views on the Western roads to the Viaduct and on the South towards the Gannel valley.
- Strong neighbourhood and community.
- Important local /corner shops on Linden Ave

<p>Key views to Protect</p>	<ul style="list-style-type: none"> • Views of the viaduct and green landscape from the upper part of Linden Crescent over the lower single storey houses at the end of the crescent. • Glimpses of the Gannel from the lanes behind Hawkins Road.  <p><i>Above: View of Viaduct and Sports Centre from Linden Crescent</i> <i>Below: View from Agar Rd Linden Ave junction over lower housing</i></p> 
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • Maintain key views by ensuring new development does not rise above surrounding housing, proposals should be tested with illustrations. • Any new development should maintain the building lines of the adjoining housing, unless that is a set-back building within an open plot, facing the street and incorporate landscaped front gardens with boundary walls. • Avoid the design of alleyways • Incorporate adequate off-street parking into layouts.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • Alleyways incorporated into design particularly around Hawkins Road/Chynance Drive development could be improved. • Small areas of grass and verges could be better maintained or much better landscaped perhaps with community group to help maintain them. • Play areas and incidental grass areas have scope for significant improvements for use and landscaping. Again, there is potential for a community group to help maintain ongoing project. E.g. the Listry Road play area and central junction green spaces. • Encourage good landscaping on front gardens to improve the public realm.

Opportunities for Improvement *(cont.)*

- Hawkins Road Flats have scope for radical improvements to the open space around them, or even scope for longer term redevelopment with more conventional affordable housing creating street frontages with private gardens.
- The south facing monopitch roof housing has good scope for an effective large-scale solar panel scheme if funding could become available.
- Plant street trees in verges if suitable space is available for selected species, roots and mature growth



Above: Hawkins Road flats set within unused and un-landscaped open space.

Below: Distinctive but untypical monopitch roofs in terrace housing.



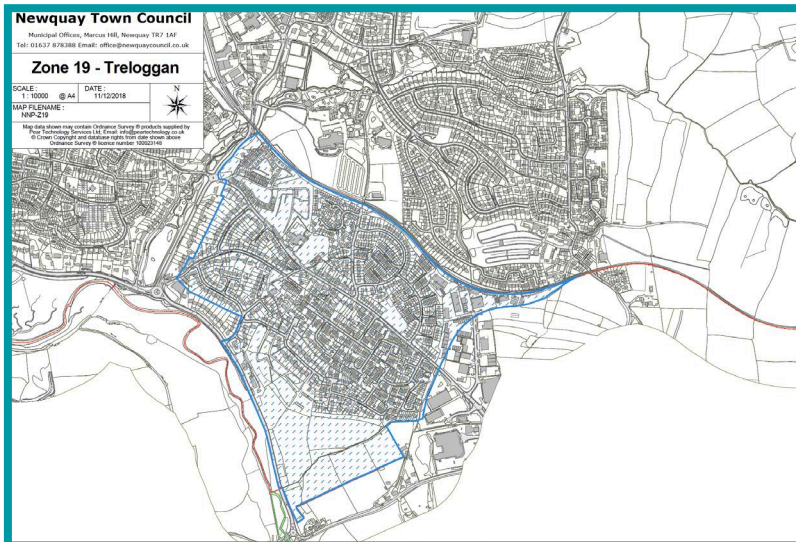
Below: underutilised green area in Trembath Crescent



19: Treloggan

General Description

Predominantly late 20th century suburban housing with a good community feel but no obvious neighbourhood centre. Centred around busy main road – Treninnick Hill/Treloggan Road providing access to Treloggan Industrial Estate



NB the industrial area has not been assessed

Important Characteristics of the area

- See volunteer survey for more detail.
- Treloggan Doorstep Green is a vital piece of community infrastructure as a linear park.
- A mixture of connected streets and cul-de-sacs set within the blocks, several cut-through footpaths evident to connect cul-de-sacs
- Primary school in good connected central location.
- Very large holiday caravan park in NW is effectively single access cul-de-sac.
- NB the area has varying building styles of different periods. But these are typical of estates all over the UK and not reflective of the Cornish vernacular.
- The area is adjacent to an industrial area along the railway including Treloggan Industrial estate and Prow Park– of which the major part is in Colan parish.

Key views to Protect

- Views from the western end of Mellanvrane Lane over the Gannel Estuary are special and made available by the lower single storey development in the area below.
- Views from the eastern end of Mellanvrane Lane look to the far side of the Gannel and green spaces around the Tavern Inn

Key views to Protect <i>(cont.)</i>	 <p><i>View across the estuary from Mellanvrane Lane</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • The general architecture of the area displays no real precedents to be followed but a more consistent unifying theme of improving the landscape in gardens and verges and constructing boundary walls on streets out of Cornish stone would be an advantage and help to make the neighbourhood more distinctive. • Retaining the general height of surrounding development, two storeys generally and lower where key views have been identified.
Opportunities for Improvement	<ul style="list-style-type: none"> • Busy main road with no obvious controlled or safe crossing points, which could be added at key points on the road or refuges. • The link from the Doorstep Green into the adjacent retail park could be made more obvious and safe as a pedestrian route.  <p><i>Hedges new and hedges old on Treloggan Road</i></p>

20a: Tretherras South: Towan Blystra estates and Trencreek


General Description

An old Farm and some houses in the SW corner formed the earlier Trencreek Hamlet now the focus of a holiday village and some 1930's housing. The majority of the area is formed by suburban estates with a mix of east/west roads and several very long cul-de-sacs. These are mostly detached 70's housing in the central area, 2 storeys and many dormer bungalows. Here the layout is spacious and suburban with verges to many routes and street trees on Billings Drive. Some parts of quite different character e.g. the eastern part of Bedowan Meadows c2005, which becomes much denser two storey detached and some townhouses. This is dominated by narrow tarmac road paths and drives with little greenery. Two green corridors along streams run through the area. To the west of Trencreek Road [outline in blue] a new development has been built with higher density, two storey houses with very strong definition and enclosure to the streets and spaces. The whole area is bounded by the railway line to the south.



Important Characteristics of the Area

- See volunteer survey for more detail.
- The two key green routes through the area are vitally important to the ecology and environmental quality. However, they have been badly integrated into the design layout since they are backed on to by rear garden walls and fences or fences to the caravan site.
- Trencreek [green boundary] still has a different character and identity, centred on Tretherras Farm and earlier housing. This is the focus of the holiday centre and caravan site.

Key views to Protect	<ul style="list-style-type: none"> Billings Drive has long views to the north up to the Narrowcliff developments. See below. 
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> Any opportunities for new or re-development that could place buildings that fronted onto the green routes would be an advantage and would improve community safety. These routes could be enhanced with landscape management and improved boundaries. Any extensions or infill development should maintain the height of surrounding houses and ensure the relationship to the street is kept in terms of gardens, walls, building lines and the location of off street parking.
Opportunities for Improvement	<ul style="list-style-type: none"> The 2005 Bedowan Meadows development could benefit from better road design as shared space or home zones. This could incorporate some design in parking control. Any opportunities for greening in this estate should be considered taking care to provide only suitable species in suitable locations. Additional tree planting on the streets, in front gardens and better definition of front garden walls in stone would all contribute to the environmental quality. Closure of the Trenchcreek Road level crossing (or restricted use) once the Newquay Strategic Route is created will make that road more a cul-de-sac and stop conflicts with excessive traffic along that narrow route between Henver Road and the A392. There will be an opportunity after the Newquay Strategic Route is created to consider a private and public realm works to create a better focal point and sense of place at the centre of Trenchcreek e.g. restoring historic buildings, boundary walls, lanes, and creating a village space or green. <div style="display: flex; justify-content: space-around; align-items: flex-end;">   </div> <div style="display: flex; justify-content: space-around; align-items: flex-end; margin-top: 10px;"> <i>Left: Tretherras Farmhouse</i> <i>Right: older houses in Trenchcreek</i> </div>

20b: Trevenson MEadows [East of Trevenson Road]

General Description

New housing 2011-2015, first of the growth area – 321 units.

Long and narrow development area on one side of Trevenson Road. Fairly dense development of 2 storey detached semi-detached and some terraces on streets and some courts. There are also some examples of 3 storey flats.



Left: Central Street and open spaces



Left: Small parking court entered by private-looking archway

Important Characteristics of the Area

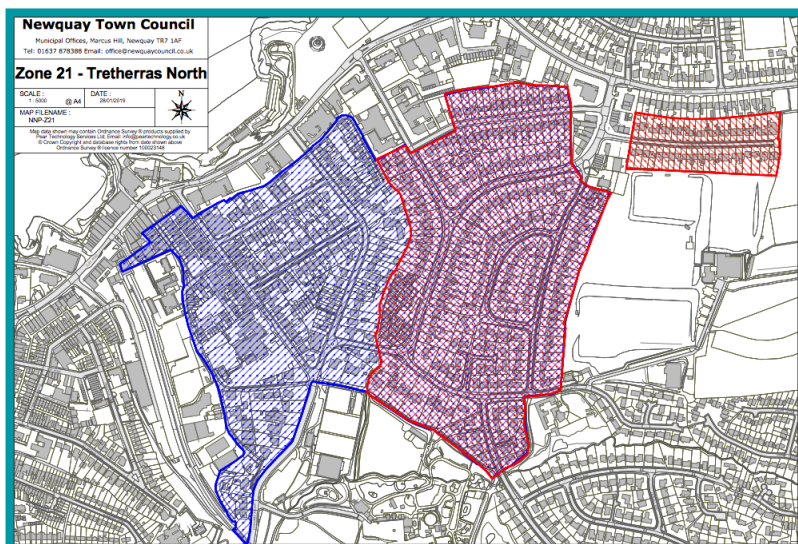
- Perimeter block development.
- Strong enclosure of streets and courts by linked buildings.
- Strong continuous frontage to Trevenson Road virtually to back of pavement, except for stretches where stone hedges have been retained or constructed.
- Green areas incorporated along main street, well overlooked and two with play equipment
- Small parking courts integrated into the development within each block many entered through private looking archway within house type.
- 3 cross streets will link to remaining growth area.
- Good landscaped flood attenuation area at Oxley Vale
- Some variation to materials render/slate cladding some limited use of stone
- Consistent design quality and variety in building forms.

Key Views to Protect	<ul style="list-style-type: none"> A rural view can be seen through the open space at the northern end of the development. However, this will be affected by some development in accordance with the masterplan.  <p><i>View through the open space at the north end of the development</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> No opportunities within this site for further development, only on adjoining land where principles of Design Code and Nansledan Masterplan should be adhered to. Control the quality of any additions to boundaries, bin storage areas, garden fences and sheds to ensure it does not impact on the whole development.
Opportunities for Improvement	<ul style="list-style-type: none"> Opportunity to increase parking spaces will be a distinct advantage only 1 per unit provided and sometimes cars park on pavements and verges where the street is not wide enough. There is potential to incorporate some within the next phase.  <p><i>Very busy on street parking can block streets</i></p>

21: Tretherras North and adjacent 10 (part) Residential Area within CSUS

General Description

Residential area. Loose grid plan. Mostly interwar semi-detached and detached houses. Some early 20th century terraces on Edgcumbe Avenue. Some areas of closely set interwar bungalows. Higher building area focused in the west and north of the area. East west roads generally run along contours. Urban greenery is key feature in enclosed front gardens.



Tretherras North area – the section within the CSUS study is outlined in blue, and adjoining, but linked, residential area in red.

Key changes since CSUS study

None identified

Newquay Pattern Book



- Edgcumbe Avenue has typical characteristics of resort residential typologies see p.22



Edgcumbe Avenue (North End)

Important Characteristics of the Area

- Eliot Gardens – good example of tree lined street
- Spacious layout mostly with landscaped front and rear gardens and grassed verges. Usually low walls enclose front gardens.
- Neighbourhood shopping focus at Henvy Road/Chester Road junction
- One and two storey buildings predominate with pitched slate roofs.

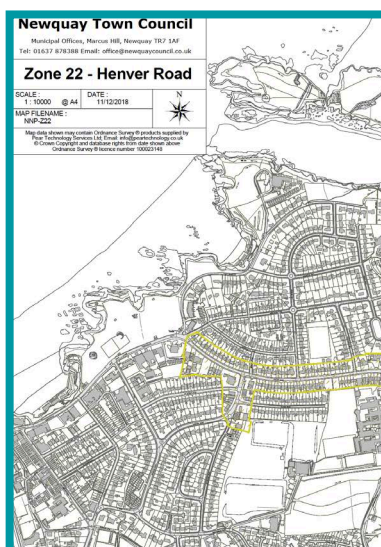
Important Characteristics of the Area <i>(cont.)</i>	<ul style="list-style-type: none"> • Pine tree at the top of Glamis Road provides a useful full stop in the view. • Edgcumbe Avenue at its South end (where it joins area 11 in the Trenance Valley) presents a very green route with landscaped front gardens on one side and tree lined Zoo and sports pitches on the other.  <p><i>Eliot Gardens</i></p>
Key Views to Protect	<ul style="list-style-type: none"> • Tretherras Road junction with Towan Blystra Road - view south and southwest of green foreground and background over Trenance and recreation areas. • Ulalia and Hilgrove Roads are open to the sky at the north end as they meet Narrowcliff and the coast.  <p><i>View of green space from Tretherras Road</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Maintain the height and form of buildings in the surrounding area for any new development. • New development in residential gardens must be carefully considered to maintain scale character and landscape quality of the area.
Opportunities for Improvement	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Neighbourhood centre Chester Road/Henver Road has many opportunities to improve the quality of the public realm, landscape and sense of place and reduce the impact of the traffic and parking dominating these busy junctions. • There are opportunities to reinforce the local treescape with additional planting of suitable species in many verges.

Opportunities for Improvement <i>(cont.)</i>	<ul style="list-style-type: none"> • Improve pedestrian crossing facilities at Hilgrove Road/Tretheras Road junction and Ulalia Road/Hilgrove Road junction. Both these are on important pedestrian routes and the current layout favours moving traffic at high speed. • Consideration of design improvement to parking /drop off pick-up facilities at Newquay Junior Academy. Also, popular routes to school such as Glamis Road • Improving key pedestrian routes to local shops e.g. by providing additional benches en-route would be beneficial to older members of the community.
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22: Henver Road

General Description


Henver road is a long gently undulating road predominantly lined with 2 storey inter-war semi-detached housing. The Chester Road junction forms a neighbourhood centre with shops and other businesses. Some occasional business uses in single storey buildings towards the east. The three and four storey apartments at the West end form an abrupt change of scale into Narrowcliff. This is an important collector road and main street linking into many neighbourhoods to the north and south.



Henver Road

Important Characteristics of the Area

- See volunteer surveys 22a and 22b for more detail.
- The shopping area at the junction of Chester Road and Henver Road is a distinctive neighbourhood centre. Focal building is the former Hawkey's Garage with its 2-storey art deco design and variety of retail uses. Other modern flat roofed buildings have been developed around it setting a characteristic theme often enhanced by some 20th century detailing.
- 2 storey semi-detached inter-war housing, along the main runs of Henver Road, are a characteristic pitched roof, built form, set back behind low Cornish stone walls and large front gardens – regular building line.
- Some older Victorian housing at the eastern end create more street enclosure as they are built closer to the road.
- The Church Street entrance to St Columb Minor is an important green gateway with a small seating area.

Key Views to Protect	<ul style="list-style-type: none"> • The views along this road itself are important and can be protected by maintaining the scale and building line along its length except at the east end where Victorian housing marks earlier development. • Looking north along Porth Way – glimpse of the sea to Porth Beach. • Trevenson Road junction has open views across school playing field to the south. • View from Henver Road [next to no. 129] through gap across lower garages to countryside beyond.  <p><i>Church Street entrance</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Any redevelopment around the Chester Road neighbourhood centre should retain and adapt the art deco former Hawkey's Garage as a local landmark. It should retain retail or complementary uses on the ground floors. Any new development of the eastern corner here should create a strong building form closer to the Chester Road street frontage but respecting the Henver Road buildings line. The art deco theme or modern design of buildings at this centre should be maintained as a strong characteristic. • Any development opportunities on the main road should seek to retain the building line, the scale and rhythm of buildings adjacent to it, the front boundary walls at the back of pavement, some planting in front garden zones, and any pre-war buildings.
Opportunities for Improvement	<ul style="list-style-type: none"> • While good improvement work has been undertaken to the public realm on Chester Road there is still scope for creative improvements to the former Hawkey's Garage to complement and bring out the art deco features. This could also include a more complementary boundary treatment to Henver Road with designed walls and planting to enhance the attractiveness of this retail area. • Any long-term opportunities to redevelop on the eastern corner of this junction could provide a bigger mixed-use building that creates a strong building form to respond to this important junction and neighbourhood centre. • Opportunities to part-pedestrianise the neighbourhood centre or give more quality space over to the pedestrians would be an advantage.

Opportunities for Improvement *(cont.)*

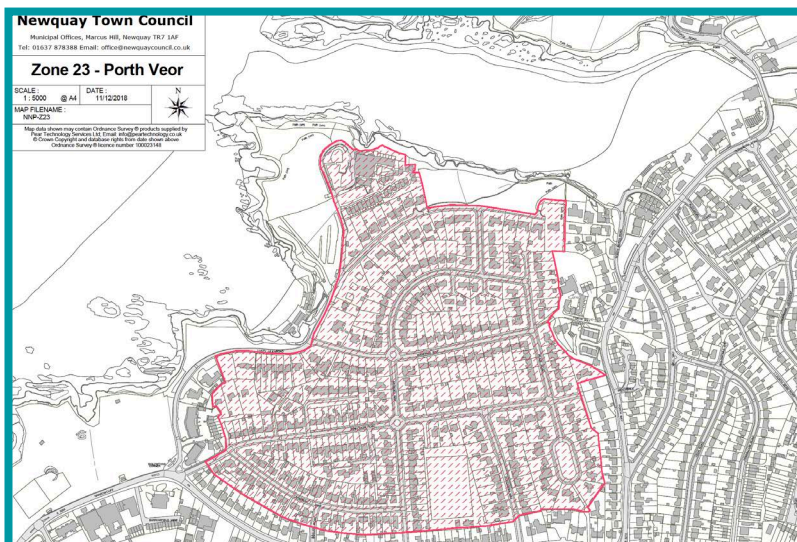


Entrances are an interesting theme in this area, with characteristic designs of different periods.

23: Porth Veor

General Description

Principally 1930's housing development in a typical layout of connected streets, perimeter blocks with some roundabouts and small circus/open space. Lusty Glaze Road has different character with stunning sea views and some larger buildings facing the Barrowfields and a run of large two storey detached houses. Most of the development are single storey units or dormer bungalows



Important Characteristics of the Area

- See volunteer survey for more detail.
- Typical 1930's layout of connected streets with pavements and verges. mostly large plots set within perimeter blocks. Detached and semi-detached housing, large front and rear gardens in perimeter blocks – all houses face the street. No trees planted in the layout.
- The majority of the housing is single storey or dormer bungalows and that is part of the areas distinctive character. Two storey housing has been developed at the west end of Bonython Road and on Lusty Glaze Road. Two larger blocks have been developed that face onto the Barrowfields – a major open space.
- Godolphin football ground located in the centre of one block.
- Circus or circular green space on Praze Road
- Larger apartment developments at western extremity of the area – these have a relationship with the Barrowfields open space. However, the new 1 Lusty Glaze development appears significantly out of scale with its 7-storey height.



Bonython Road

<p>Key Views to Protect</p>	<ul style="list-style-type: none"> • Lusty Glaze Road enjoys stunning views of the sea, Barrowfields, Lusty Glaze Beach and Porth. The Glendorgal Hotel sits on the promontory but is set on lower ground and does not feature much in longer views with its holiday development to the rear. • The views of the built edge of the settlement that faces onto the coast and open publicly accessible spaces is vitally important to the character and attractiveness of the town.  <p><i>West end of Lusty Glaze Road overlooking the Barrowfields</i></p>
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • It is essential that the development of buildings along the edges of the settlement that face the coast retain a locally distinctive character in the use of materials, scale, rhythm, architectural form and detail. This can be achieved with contemporary design as well as a more local vernacular. We need to ensure any further developments can help to reinforce that local distinctiveness. • A pattern of building scale has emerged on Lusty Glaze Road with two larger scaled buildings on the stretch facing the Barrowfields, three storeys in an intermediate zone and then returning to two storeys on the main stretches facing the coast. It is important to avoid any further overscaled development along Lusty Glaze Road and to retain this reducing scale. • ‘One Lusty Glaze’ should not be taken as an appropriate precedent for building height in this area especially as it can be seen rising above the surrounding roofscape from many longer viewpoints around the settlement. • Replacement or rebuilding of any houses on Lusty Glaze that road should follow the existing building line, set behind a front landscaped parking area or garden, bounded by a Cornish stone wall at the pavement. • The rhythm of existing properties and plots should be reflected in the building form of any replacement development especially if they are wider plots. • To help reinforce the local distinctiveness it would be an advantage to include some element of pitched roofs in the design, with materials including natural slate and local stone and render.

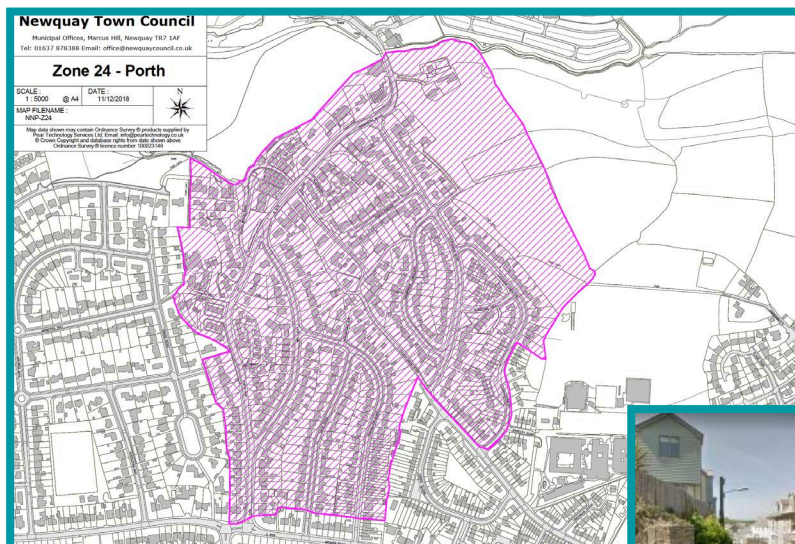
Key Design Principles to adopt in the area <i>(cont.)</i>	<ul style="list-style-type: none"> • Replacement buildings within the internal streets of this area should retain the scale, building line and built form of the existing housing. Retain the characteristic layout of streets with grassed verges and open areas such as Praze Road central green space and Godolphin football ground. • Any conversion of front gardens into parking areas should be carefully designed to partially screen cars and incorporate quality landscaping.
Opportunities for Improvement	<ul style="list-style-type: none"> • An active policy to guide and manage change along the coast road will be important as development pressures increase in this highly valuable location. • Additional landscaping suitable for the marine environment would enhance many areas including roundabouts verges and private gardens. <div data-bbox="580 781 1219 1090" data-label="Image"> </div> <p><i>Lusty Glaze Road with modern detached two storey houses using some locally distinctive materials and reflecting the typical building plots.</i></p>

24: Porth

General Description

Housing areas overlooking Porth Beach and stretching southwards to Henver Road. Includes Lewarne Estate of 1950's semi-detached pitched roof bungalows. Scattered detached houses on the Coast – Alexandra Road.

Porth was a historic port, the green lane of Porth Bean road leads into its centre.



Porth Bean Road – emerging from under the Alexandra Road bridge

Important Characteristics of the Area



- See the two volunteer surveys for more detail. 24a and 24b
- Incorporates Porth Promenade and Porth Beach – the focus of the area.
- Porth Bean Road – an important historic green lane, leads into the historic core of the area, passing under the more recent Alexandra Road, it retains groups of historic buildings [2 storeys] along its route and links into Beach Road.
- Porth Lodge Tavern half way along this lane. Converted barns part of original farm.
- Most housing takes advantage of the topography and is laid out to maximise coastal views e.g. Lewarne Estate
- Alexandra Gardens off Alexandra Road
- The buildings at the southern end of the streets close to Henver Road are 2 storeys while the main buildings along them are single storey.



Key Views to Protect

- All the main residential roads run down the hillside, looking north present good views to the sea, and headland. Allowed by low bungalow development and wide roads with verges.
- Alexandra Road, which is raised, permits good views across the lower rooftops across Porth Beach towards Whipsiderry and Trevelgue Headland.

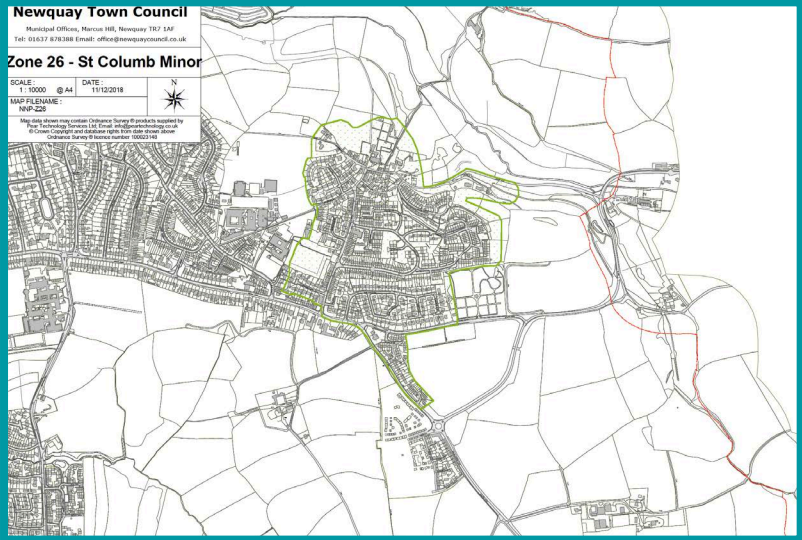
<p>Key Views to Protect <i>(cont.)</i></p>	<ul style="list-style-type: none"> The view from Beach Road across the open garden space to Porth Beach and Whipsiderry beyond.  <p><i>Lewarne Road - one of many north/south streets with views of Porth Island and the sea.</i></p>
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> Design any new buildings at a consistent height with their neighbours to make the most of stepping down the hillside and maintaining good views to the beach sea and headland – consider the impact on views from the south and eastern parts of this area over the low rooftops. Retain any pre-war historic buildings and restore their characteristic features where possible, including walls and hedges on the boundaries. Any new infill should seek to retain the enclosure of street and active frontages onto the street. Maintain the landscape and historic character of Porth Beach Road – a narrow quiet green lane in the heart of this area. Avoid any loss of its green edges.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> Improvement of Alexandra Gardens as a local asset.  <p><i>View along Beach Road to Porth Beach and Trelvague Head</i></p>

25: Porth Bean

<p>General Description</p>	<p>This is a residential area with a primary school and secondary school at its heart. The housing is varied with a distinctive “Cornish Home” estate around Cross Close, a range of pre-war terraces and 1950’s Council housing plus private homes. Virtually all housing is of two-storey height. Porth Bean Road runs diagonally through the area. A narrow footpath runs between the schools from this road.</p>  <p><i>Cornish Homes on Cross Close</i></p> 
<p>Important Characterisitcs of the area</p>	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Wide grassed areas around the entrance to Duchy Ave with Grade II listed stone cross. • Distinctive Cornish Home estate with internal community green. • Most properties have good sized gardens. • Green footpath link between the schools leading to St Columb Minor and the pre-school. • Play area hidden between the backs of Coronation Way and Treviglas School
<p>Key Views to Protect</p>	<ul style="list-style-type: none"> • View down Porth Bean Road (towards Porth) from the junction with Lewarne Road • View of the spacious entry to Duchy Avenue

<p>Key Views to Protect <i>(cont.)</i></p>	 <p><i>Junction of Porth Bean and Lewarne Roads gives longer views to the sea and leads into the green lane that Porth Bean Road becomes.</i></p>
<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> • Maintain the scale and building line and front garden boundaries to any new housing. • New development and alterations should seek to improve the public realm, the character of the streets, courts and open spaces.
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> • The area has extensive grassed verges and spaces that are not well landscaped or used and often overrun for parking. Consideration could be given to redesigning some of the street spaces to make parking easier and more efficient and creating landscaped green spaces that could contribute more to the local environment. • Despite what appears to be large parking areas, there is conflict in parking, with Cross Close difficult for two-way traffic, exacerbated at school pickup and drop-off times. Control of parking on certain streets especially school bus routes may ease blockages and conflicts. • Consider how to improve the benefits and use of the green space in the centre of the Cross Close development. E.g. community group use, food growing space, better designed play space.  <p><i>The wide entrance and underused green spaces on Duchy Ave.</i></p>

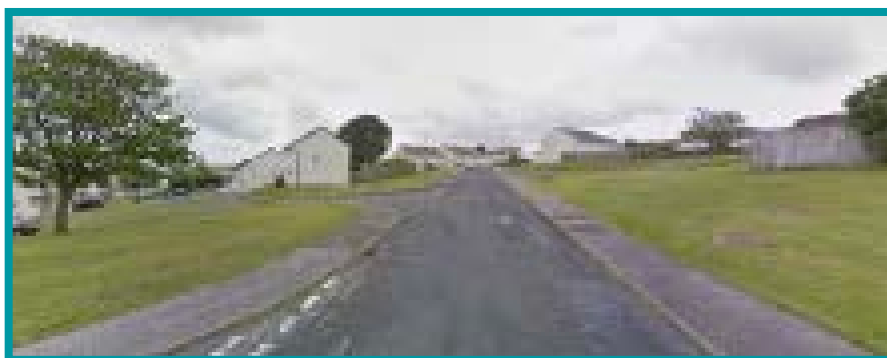
26: St Columb Minor

<p>General Description</p>	<p>A historic village centre with notable parish church and 18th century buildings. The land to the south of the old village is filled with small late 20th century estates up to the A3058 and A3059. The historic core is a designated conservation area.</p> 
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> • Village High St p17 • Parish Churches p14 • Cottages p44,45 • Garden Lanes p25
<p>Important Characteristics of the area</p>	<ul style="list-style-type: none"> • See volunteer survey for more detail. • The historic core is a very significant area and recognised by its conservation area status. • Institute Square. NE of church with historic terraces using the same building stone – Elvan. 3 listed buildings and The Institute. • The parish church is an outstanding feature and landmark – it is visible for practically the whole length of Church Street the main street of the village. • Recreation ground, just outside boundary and entrance on Church Street. • It is notable that the surrounding housing estates have missed the opportunity to reflect the special characteristics of the historic village on their doorstep. • Characteristic small village lanes e.g. Stanways Road, Parkenbutts, Church Lane.

Important Characteristics of the area <i>(cont.)</i>	<ul style="list-style-type: none"> • The green lane of Priory Road – connected to the Porth Valley and semi natural woodlands. • Rialton Heights green – potential for improvements <div data-bbox="571 331 1428 517">  </div> <p><i>Left: Institute Square. Centre and Right: historic stone terraces</i></p>
Key Views to Protect	<ul style="list-style-type: none"> • Views of the church tower are very important from all approaches to the village and its centre. • From the northern part of the village it is possible to see the prehistoric tumuli on the sea cliffs between Whipsiderry and Watergate Bay. Porth Valley is also visible from the northern part of the village. • From the School Close estate St Agnes Beacon and Carn Brea is visible on a clear day. • Town views are visible from School Close, properties on A3058 and the upper end of Parkland Close. <div data-bbox="571 1120 1401 1339">  </div> <p><i>Left: Views to Newquay. Centre: Church tower. Right: views towards Porth</i></p>
Key Design Principles to adopt in the Area	<ul style="list-style-type: none"> • The special character of the historic core – the conservation area – should be recognised and protected. Key features are the grouping of buildings, the tight enclosure of streets and spaces, the building forms and roof shapes, the proportions of windows, boundary walls plus locally distinctive building materials and details. • Any future infill development or building or landscape improvement projects may help with a gradual increase in local distinctiveness if they begin to reflect the characteristics of the village core. • No further historic fabric should be lost but should be restored and repaired as an asset in new improvement or development schemes.

Opportunities for Improvements *(cont.)*

- A conservation area appraisal and management plan is long overdue for the village and could highlight many valuable characteristics and opportunities for improvement.
- Opportunity to reinforce the historic character of street enclosure could be taken by introducing more substantial stone walls and landscaping at the Farmers Arms car park.
- A sensitive public realm improvement scheme would enhance the historic character and attractiveness of Institute Square and Priory Road. This could involve undergrounding the unsightly overhead cables in the conservation area.
- Improvements to Leader Road junction with Church Street would enhance the setting of the Chapel and a small green space at the entry to this estate.
- The Leader Road estate has a huge amount of poorly used grassed spaces with exposed rear fences and may be improved with selective new development and well-designed public spaces and gardens

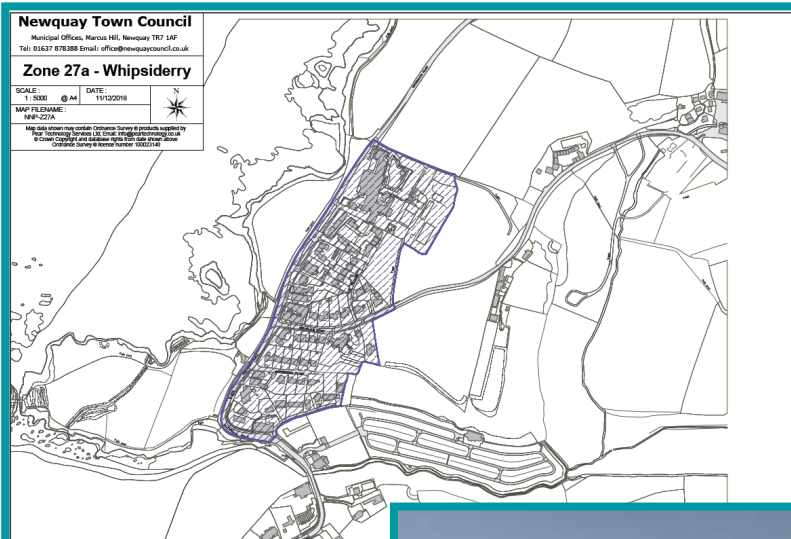


Expanses of poorly used space and layout in the Leader Road estate quite out of character with the historic rural settlement.

27a: Whipsiderry

General Description

A linear development running north from the Porth Valley and situated on the east side of the B3276, with the exception of one Grade II listed cottage. Sea cliffs to the west of the road with the South West Coast Path and access steps to Whipsiderry Beach. This area can be seen from many viewpoints north of Henver Road outside this area.



Listed building on Watergate Road with tall apartment block standing out behind.



Important Characteristics of the area

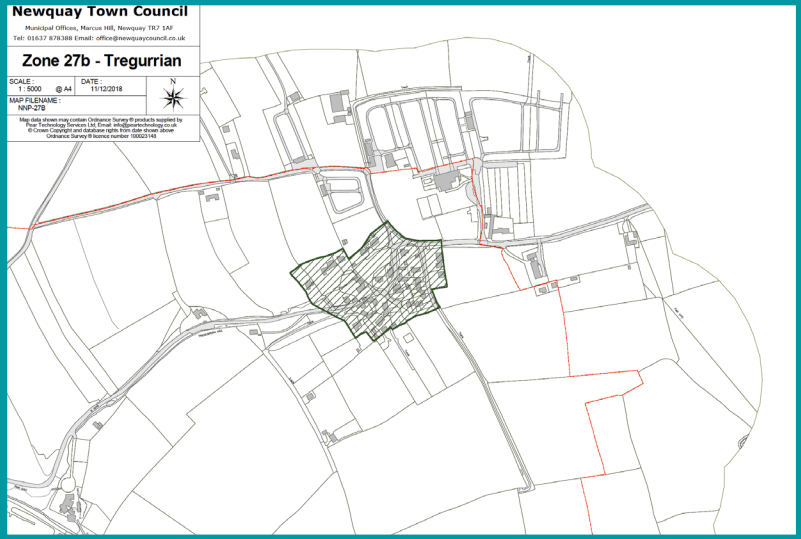


- See volunteer survey for more detail.
- Setting on the cliff edge overlooking Porth is the areas main characteristic.
- The design quality of the buildings is generally poor with little attempt to develop a neighbourhood character in the planning of the area.
- 2-storey houses and bungalows fronted on Watergate Road by apartments of 3-storeys. One apartment development stands out incongruously at 4-storeys on the corner of Trevelgue Road
- View from the higher ground to Porth and Newquay



View of Whipsiderry from the north with blank gable of Sands Resort standing out in the landscape

Key Views to Protect	<ul style="list-style-type: none"> • Views out from Watergate Road of the coast, • Views down Watergate Road and from Trevelgue Road of Porth and the undeveloped valley behind. NB St Columb Minor tower is just visible
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> • Retain maximum height of 3 storeys for any additional apartment development on Watergate Road. • Retain 1-2 storey height in residential areas behind the Watergate Road frontage buildings • Avoid the use of extensive areas of bolder colours in painting or self-colouring render. • Integrate more locally distinctive materials such as slate and local stone for elevations and boundary walls.
Opportunities for Improvement	<ul style="list-style-type: none"> • Guidance on maintenance and detailing of new buildings may improve their weathering and resilience in this exposed setting.

27b: Tregurrian

<p>General Description</p>	<p>A small hamlet with a central focus point in the vicinity of Merbian Farmhouse. The B3276 runs through with a couple of cul de sacs, one either side. This is approached from Watergate Bay to the south.</p> 
<p>Important Characteristics of the area</p>	<ul style="list-style-type: none"> • See volunteer survey for more detail. • Grade 11 listed cottage - Ivy Cottage. 18th Century with 19th Century alterations. • Small grouping of historic buildings in centre but split by B-road. Additional buildings added in varied styles in modern materials with no real attempt to reflect local distinctiveness in layout street enclosure or building forms.
<p>Key views to protect</p>	<ul style="list-style-type: none"> • Distant sea views to the south from the centre of the hamlet unlikely to be screened by new development but landscaping walls garaging etc may affect. <div style="display: flex; justify-content: space-around;">   </div> <p><i>Historic buildings could be used as a model for any new house opportunities, and these should be carefully maintained.</i></p>
<p>Key Design Principles to adopt in the Area</p>	<ul style="list-style-type: none"> • Consider opportunities to develop more enclosed street frontage and closer groupings of buildings similar to the original farm buildings. • Use traditional building forms, and locally distinctive materials. Avoid large areas of bolder colours in painting of elevations.


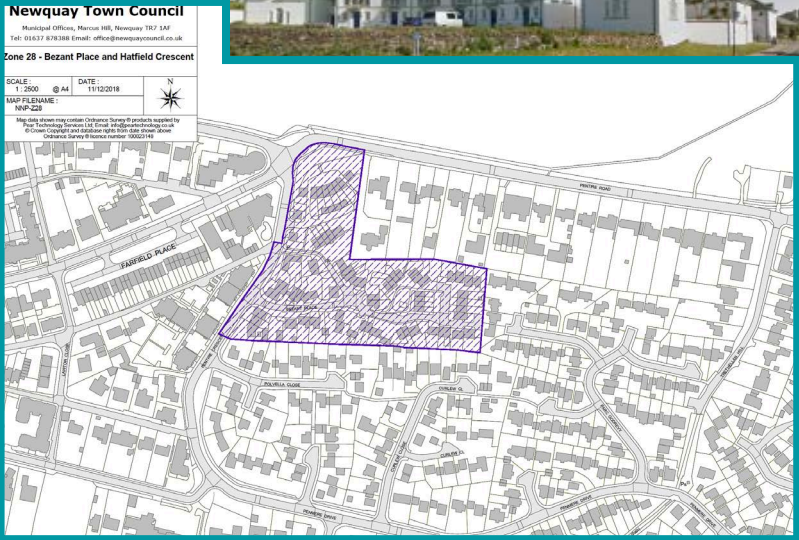
Opportunities for Improvement


- Create more enclosed yards, gardens and parking areas by building some higher stone walls, or Cornish hedges in order to tie together the varied architectural styles and differing layout of plots.
- Consider creation of narrower gateway features or varying surfacing to slow traffic down in centre of hamlet.



Modern estate housing looks incongruous next to historic core, but stone walls and hedges may help tie together the hamlets appearance.

28: Bezant Place and Hatfield Crescent

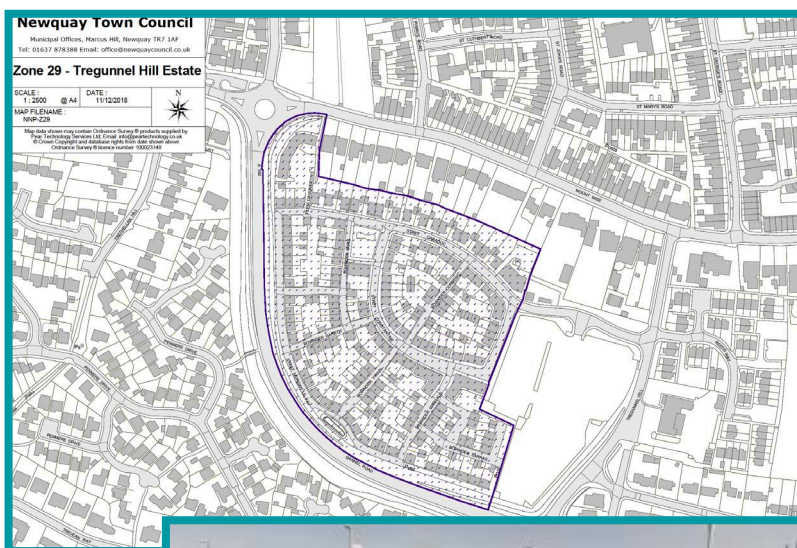
<p>General Description</p>	<p>This was the earliest development on Duchy owned land in Newquay c2005. It consists of a curved terrace on the prominent corner of Pentire Road, behind which is an elongated cul-de-sac with small enclosed green.</p>   <p><i>Hatfield Crescent and Bezant Place development: corner of Pentire Road and Pentire Crescent</i></p>
<p>Newquay Pattern Book</p>	<ul style="list-style-type: none"> The design of this development may have been a precursor to the Newquay Pattern Book.
<p>Important Characteristics of the area</p>	<ul style="list-style-type: none"> Formal gateway expressed through matching 3 storey buildings. 3 storey curved terrace with wings marks the important corner overlooking Fistral Beach More informal layout internally suggests small village with central green space. The cul-de-sac is designed to incorporate a series of enclosed spaces or places. There is one other pedestrian link back to Pentire Crescent. Some properties front directly onto the road but some incorporate stone hedge front or side boundaries.
<p>Key Views to Protect</p>	<ul style="list-style-type: none"> The private shared space in front of Hatfield Crescent enjoys views over Fistral Bay but there are no significant views from public spaces in the development. The framed view out through the pair of gateway buildings on Bezant Place has already been closed by an uninspiring façade.
<p>Key Design Principles to adopt in the Area</p>	<ul style="list-style-type: none"> There is little scope for further development in Bezant Place but any extensions should be designed to reflect the classical or vernacular architecture adopted here. Any further intensification of development should ensure adequate parking can be provided without exacerbating any existing pressures on the street and open space.

<p>Key Design Principles to adopt in the Area <i>(cont.)</i></p>	<ul style="list-style-type: none"> • There is little scope for further development in Bezant Place but any extensions should be designed to reflect the classical or vernacular architecture adopted here. • Any further intensification of development should ensure adequate parking can be provided without exacerbating any existing pressures on the street and open space.  <p><i>Bezant Place: Central unadopted green</i></p>
<p>Opportunities for Improvement</p>	<p>Bezant Place</p> <ul style="list-style-type: none"> • The absence of a management company has led to some issues, since the green has not been adopted by Cornwall Council and the original playboat has been removed due to lack of maintenance.

29: Tregunnel Hill: Duchy site

General Description

This is a development of about 174 dwellings with some employment space formed around a network of streets and courts. It is based on the principles established for the later Nansledan development but applied to the context of this location. It is a wedge-shaped development running along the north side the Gannel Link road and was built 2016-2017. To the East side is the Cornwall Council owned car park.



Tregunnel Hill: corner of Mount Wise and Gannel Road

Newquay Pattern Book

- The Newquay Pattern Book [by Adam Architecture 2006] is a non-prescriptive source of typologies and characteristics from the Newquay area that have informed the design of this development.

Important Characteristics of the Area

- This site is designed as an urban infill scheme, a piece of a larger townscape.
- It has a network and hierarchy of primary streets mainly running east west along the contours with secondary streets, mainly north south running between them. Back courts are located within the blocks of these grids and are envisaged as more semi-public spaces as the focus for car parking edged by garages and back garden walls.
- Pelleas House, the larger scale building on the Mount Wise / Gannel Road corner responds to this large roundabout although is screened behind a substantial tree buffer.
- There is a very strong enclosure of the main streets, with buildings fronting directly onto the pavement and connected by high walls.
- Gannel Road has a successful an attractive component that faces outwards to main the main road but is safe and quiet by virtue of creating a one-sided lane edged by a stone hedge and tree screen.

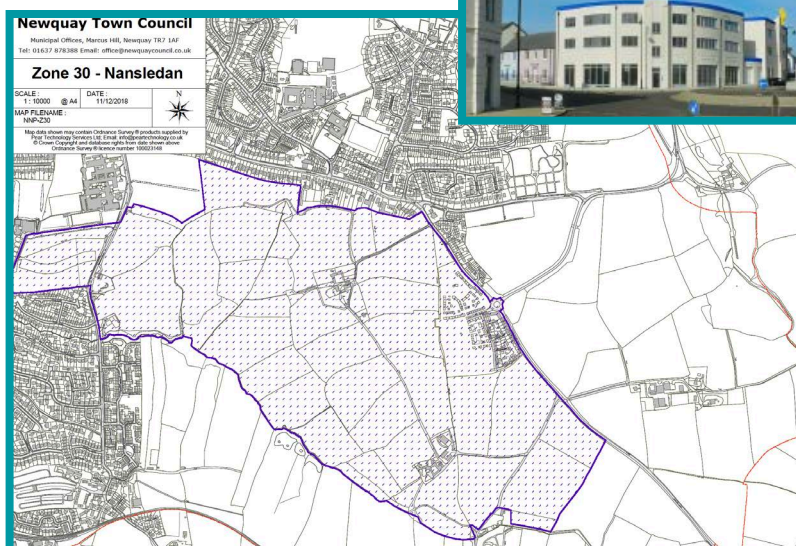
Important Characteristics of the Area <i>(cont.)</i>	<ul style="list-style-type: none"> Wide range of traditional or vernacular materials used, including diminishing course slate, roughcast render, timber cladding, painted brick, stucco, killas stone, steel profiled sheet, slate hanging <p>Note: A lot of the characteristics of street enclosure, classical and vernacular architecture and careful location of car parking is not characteristic of Newquay developments after the Second World War. It has been based on the work of the Newquay Pattern Book which has focused on more historic [pre-war] developments in identifying its typologies.</p>
Key Views to Protect	<ul style="list-style-type: none"> There are views down the radial secondary streets framing the landscape beyond.  <p><i>Tregunnel Hill – framed landscape views at the end of the secondary streets</i></p>   <p><i>Tregunnel Hill: Left outward facing street with stream and landscaped route. Right: Inner tightly enclosed street.</i></p>  <p><i>Tregunnel Hill: Internal parking and pedestrian spaces</i></p>
Key Design Principles to adopt in the area	<ul style="list-style-type: none"> There is little scope for further development in Tregunnel Hill. Minor alterations are normally controlled by a Design and Community Code, an agreement between the owner and the Duchy of Cornwall and should reflect the key characteristics of the architecture and development form. Any additions or extensions should seek to ensure the parking demand does not increase without providing additional spaces.

Opportunities for Improvement	<ul style="list-style-type: none">• Consideration of a residents parking scheme could be beneficial, as some areas appear to suffer from increased non-residents parking in the summer.• The back lanes or courts are not very private areas, with pedestrian through routes but are also not well overlooked often enclosed by garages or high walls. Opportunities to close some as pedestrian through routes or allow some alterations to create more overlooking may improve these spaces.• While the general build quality is very high there is some concern with certain materials and their longevity in the marine climate. For example, rusting metalwork is seen in some doorbells and letterboxes.
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30: Nansledan

General Description

Nansledan – or “Broad Valley” is the Newquay Growth area. It is a town-scale, mixed use development which, over time, will grow to c4000 dwellings. In addition, it will provide a school, employment spaces, green areas, local shopping, neighbourhood centres, food growing and community facilities. Nansledan will be developed over many years by different housebuilders but will conform to its strict masterplan and design codes. The first phase, Trewolek, started in 2014 and the second phase, Kosti Veur, is underway See <http://nansledan.com/about/the-masterplan/> This development will eventually be comprised of many character areas or neighbourhoods.



Nansledan: The gateway to the new main street and Quintrell Road

Newquay Pattern Book

- The Newquay Pattern Book [by Adam Architecture 2006] is a non-prescriptive source of typologies and characteristics from the Newquay area that have informed, and should continue to inform, the development of Nansledan. It is used in conjunction with published design codes and relevant masterplans.

Important Characteristics of the Area

- The Nansledan masterplan is established on a town scale, comprised of distinct neighbourhoods and encouraging community identity. The masterplan includes mixed use development with larger scaled commercial and retail buildings at important nodes in its layout to benefit from accessibility and encourage activity. It incorporates many community facilities throughout the development.
- The scale of buildings and density of development reflects the local importance of these focal points as does the scale and design of the public realm.

<p>Important Characteristics of the Area</p>	<ul style="list-style-type: none"> • A high degree of accessibility and interconnectivity is established through the development of a connected street network. There is a hierarchy of main streets, secondary streets and quieter green lanes and courts. The secondary streets tend to follow the contours while the main streets and green lanes generally run downhill. • The Newquay strategic link is incorporated as a traffic controlled main street, not a by-pass, to ensure it brings life to the community. • The existing phases are two of the outer development areas forming the northern part of the link road. These form strong building frontages facing onto Quintrell Road, three storey mixed use blocks create the corners to the primary roundabout. • Quintrell Road is created with buildings positively edging the street. An ornamental seahorse attached to a three-story mixed use has generated some debate. • The street design is characterised by variations in paving and kerblines at important junctions to slow traffic and give a greater sense of priority to pedestrians. • Variations of building design do respond to different contexts and locations adding to the areas legibility. • It is a key characteristic that the architectural design has sought to reflect only historic development. The pattern book upon which it has been based has only focused on certain pre-war typologies. It does not incorporate any modern or contextual contemporary design.
<p>Key Views to Protect</p>	<ul style="list-style-type: none"> • There will be many important views from publicly accessible locations in this large development as it proceeds. Many will be focused down the public routes and streets that drop down into the valley over the Chapel Stream to countryside beyond. • Quintrell Road currently has views from its public frontage, but these will change as new development proceeds on the north eastern side of the road. <div data-bbox="580 1664 1222 2024" data-label="Image"> </div> <p><i>Nansledan: Quintrell Road</i></p>

<p>Key Design Principles to adopt in the area</p>	<ul style="list-style-type: none"> The design principles as specifically set out in the Masterplan, it's separate Building Code, and informed by the Pattern Book. These cover strategic principles to detailed design guidance including landscape and materials. These principles will be safeguarded by the Duchy but are also tied into the planning permissions.  <p><i>Nansledan: Housing of varying scales but always creating a strong street frontage. On this main street the buildings are slightly set back behind railings which form the edge to the back of pavement.</i></p>
<p>Opportunities for Improvement</p>	<ul style="list-style-type: none"> Care should be taken to limit pedestrian movement through back courts or parking courts, making them more private, easier to recognise strangers and therefore more secure. Careful consideration should be given to parking numbers and avoiding issues that are occurring at nearby Trevenson Meadows where pressures bring additional parking on the streets causing obstructions and overrunning of landscaped areas. Potentially increasing parking provision may be a benefit in later phases.